

# Chapter 8: Urban Design Strategy

## 8.1 Introduction and Policy Context

### Introduction

This chapter of the LPF brings together the objectives of the previous chapters, focusing on their contribution at both the site and wider plan level to a sustainable and coherent approach to urban design. It does this by setting out overarching objectives relating to urban design and by applying the objectives of the previous chapters through to the different scales of the framework and opportunity sites.

The Strategy draws on the unique assets of Clondalkin to ensure that new development respects the historic character and key features within the historic core of the village, through a careful approach to placemaking. In the wider environs principles for development are set out to ensure that growth reflects both its geographic and planning policy context

The preparation of the Urban Design Strategy for the Clondalkin Local Planning Framework involved the carrying out of a baseline assessment of the LPF area and using the findings and outcomes of the pre-plan consultation to inform a Strengths, Weaknesses, Opportunities and Challenges (SWOC) assessment (see Chapter 2).

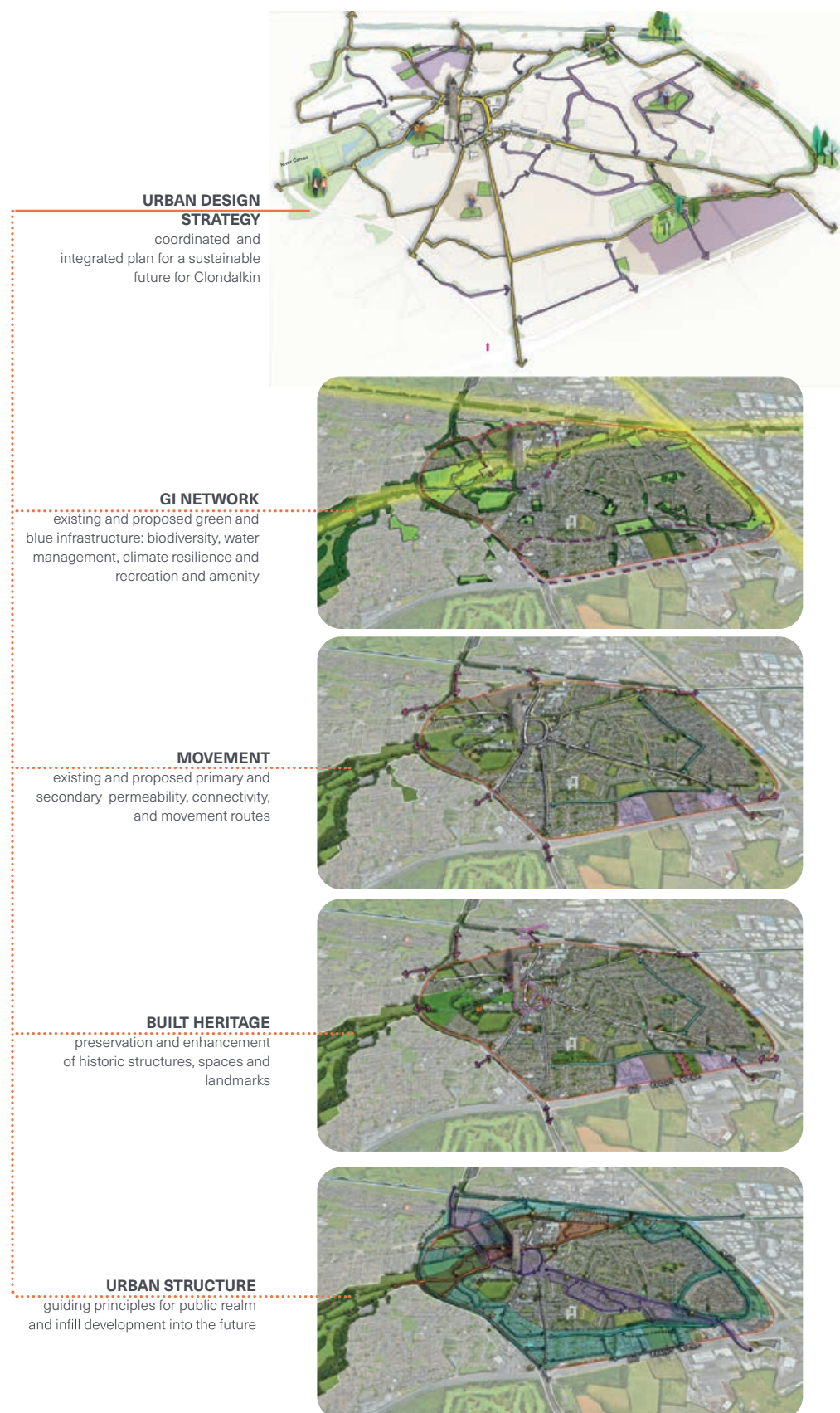
The conclusions of this work indicated that the strategy needed to address the key themes of Urban Structure, Built Heritage, Movement and Green Infrastructure at both the strategic and local level. Therefore, the first step was to identify an approach for each of these themes across the entire LPF area based on the opportunities and challenges falling out of the SWOC analysis. This approach has been reflected in the previous chapters. The different themes in each chapter have then been integrated to create the Urban Design Strategy (See Figure 8.1).

The individual sites and frameworks set out further in this chapter will bridge the different scales of the strategy. Larger framework sites are described in sufficient detail to guide their sustainable development in the short to medium term. A variety of smaller scale opportunities are also identified and described. Some of these can become early wins or pilot projects. Others are longer term objectives and opportunities, intended to guide development management into the future.

### Policy Context

The LPF and the urban design strategy is set within a wider policy context as outlined in Chapter 1 and the other chapters of this document. It is also set at a time of a recognised national housing crisis. Different measures by the government in response to this crisis will be of relevance to growth in the area. The application of densities set out in the Sustainable Residential Development and Compact Growth Guidelines applies to all development and is relevant to Clondalkin as set out in Chapter 6. The Planning Design Standards for Apartments issued on 8th July 2025 will apply to all planning applications made to the Local Authority following that date.





**Figure 8.1:** Layers of Urban Design Strategy

This strategy recognises the need for compact and sustainable growth within Clondalkin and sets out the design parameters, which take account of the place specific context of Clondalkin, to guide sustainable development over the next decades of its history.

## 8.2 Integrated Design Approach

At each scale, consideration is given to urban structure, movement, built heritage, and green infrastructure, examining opportunities for improvement and ensuring that as Clondalkin grows the key assets which create its sense of place and identity are sensitively considered.

### Movement

Urban structure is built around movement and safe and convenient active travel routes are critical to this. Streets and public spaces are where public life takes place and opportunities to improve these spaces and create new ones are explored building on objectives in Chapter 5.

### Built Heritage

Protection of the built heritage of Clondalkin is embedded into the strategy reflecting the objectives in Chapter 7. The historic road network, ACAs, protected buildings, structures of historic interest and cultural identity of Clondalkin is integrated into each scale and identified areas within the LPF. This allows for greater legibility of the built heritage.

### Green Infrastructure

As shown in Chapter 4, gaps have been identified within the green infrastructure network within and around Clondalkin. The urban design strategy takes account of these and identifies where they can be overcome or improved within various areas of the strategy.

In addition to the above, where particular infrastructure or community facilities are identified as important to provide, they are identified in the relevant framework site.

### Overarching Urban Design Strategy Objectives

Taking account of the vision and strategic objectives set out in Chapter 2, and the themed detailed objectives within each chapter, the Urban Design Strategy seeks to deliver on the following aims in the context of delivering quality and sustainable planned growth for Clondalkin;

- protection and enhancement of the cultural and built heritage,
- delivery of sustainable and more efficient movement options for everyone, and
- enhancement of green and blue infrastructure.

This approach also responds to the wider Clondalkin area being designated as a decarbonisation zone (See Chapter 3), with an aim to cut emissions in the area by 51% by 2030.

These overarching strategic urban design objectives, set out below, should be read alongside the detailed objectives set out within each chapter of the LPF and the specific context to which they are applied within the different framework sites.

### Protection and Enhancement of the Cultural and Built Heritage:

- Enhance Clondalkin's sense of place and identity through recognition and respect of the existing cultural and built heritage context and protection of its key historic features
- Require development to respond positively to vistas, landmarks, and the setting of protected structures and architectural conservation areas (ACAs) within the Plan area.
- Ensure that public realm, frontages, open space and streets within the Plan area whether in new development or in the redevelopment of existing built form are designed to provide a quality public realm and enable the safe and comfortable movement of all users, extending and enhancing natural heritage corridors where feasible.
- Ensure that new development strengthens the urban fabric through high-quality, well-designed development and interventions which respond to the urban form, urban grain, and layout in the context of Clondalkin and ensure that the approach to building height, materials and finishes are appropriate to this context.
- Promote Climate Action by supporting the adaption and re-use of the existing building stock as a compact growth model that preserves local identity while encouraging new investment in the core area and creating a more attractive environment for residents and visitors.
- Identify a village enhancement scheme for delivery which will set the context for future village public realm upgrades through the consistent use of appropriate materials which enhance the overall identity of the village centre
- Reduce visual clutter, ensuring that signage, street furniture, and road markings, within the village centre and surrounds are simple and visually restrained in design promoting a holistic approach to the public realm, promoting SDCC Shop Front Design Guide for shopfront signage.



### Sustainable and More Efficient Movement Options for Everyone:

- Ensure that areas of concentrated new growth are well connected to the town centre and to each other so that they add to, rather than draw from, its vitality.
- Ensure the maintenance of access for destination traffic and businesses within the town centre while deterring through traffic that contributes to traffic problems within the Plan area.
- Provide access to and enjoyment of the built and natural heritage assets of Clondalkin within the movement framework to encourage their enjoyment by the whole community.
- Integrate public transport services within a safe and attractive network of cycling and walking routes to and between established communities in the Plan area.
- Support pedestrian and cyclist permeability and connectivity and address physical severance with priority given to improving routes to schools as a measure to improve safety, encourage physical activity and reduce congestion at school opening and closing times.
- Deliver on a cohesive strategy for improved public realm and associated wayfinding and legibility measures.
- Promote Climate Action by reducing the need for car-based transport where feasible, facilitating active travel and public transport to reduce emissions, create a safer and more pleasant environment and improve options for healthy activity.

### Enhancement of Green Infrastructure:

- Recognise the existing green infrastructure of the Plan area (the River Camac, the Grand Canal, Knockmitten Park and Clondalkin Park) as the foundation of a green and blue infrastructure network, reinforce and create improved links between them and new development areas, using to best effect the opportunities presented by the framework areas and taking account of the gaps and opportunities identified in Chapter 4.
- Extend the consolidated core green areas (including the framework sites) into a network of secondary corridors and biodiversity stepping stones across the existing residential communities and key streets within the Plan area (Monastery Road, New Road, Orchard Road, Old Nangor Road) to conserve and create ecological links.
- Enhance the attractiveness of Clondalkin by further contributing to quality placemaking through improving the opportunity for the connection of humans with nature.
- Identify underutilised or ancillary green spaces within the Plan area appropriate for inclusion in the extended green network whose improvement would allow not only for the promotion of biodiversity but also for the integration of nature-based solutions for the management of urban drainage and the promotion of active travel and healthier lifestyles.
- Increase public open space provision in the Plan area through the development of the framework sites to include an improved, universally accessible green corridor at Knockmeenagh Lane and the provision of a quality public open space designed to cater for both passive and recreational uses and Green Infrastructure links through the site at 9th Lock Road.
- Promote Climate Action by identifying flood measures and responses critical to Clondalkin's resilience in adaptation to and mitigation of climate change and integrating nature-based solutions for surface water management into a wider urban greening strategy that promotes biodiversity, or the use of alternatives such as green or blue roofs and green walls where such solutions are not practicable at ground level.

### Deliver quality and sustainable planned growth for Clondalkin:

- Facilitate an appropriate spatial distribution of new development, across the Plan area, including the Framework sites, that supports the creation of new linkages, increased green infrastructure, public realm, and opportunities for economic activity.
- Deliver residential growth and supporting infrastructure that not only meets housing targets but also promotes quality as part of a variety of house types and tenures, having regard to relevant guidelines or planning statements.
- Optimise densities across the Plan area so that the mix and intensity of land uses are appropriate to their location in the town and their historical context and to their access to facilities, amenities, and public transport.
- Encourage compact growth within the Plan area through the reuse of derelict and infill sites in line with best practice urban design and conservation principles, including sites that can be used to effect regeneration of the built heritage.
- Support higher density redevelopment of brownfield, derelict and infill sites, where appropriate to context, to include mixed-uses for retail, services, tourism, community, and employment creation. Height will be guided by SDCC Building Height and Density Guide 2022, adopted as part of the South Dublin County Development Plan 2022-2028.
- Promote and enhance a diverse and resilient local economy that enhances existing business strengths including, tourism, services and retail and the maximisation of commercial occupancy.
- Encourage positive community engagement and a vibrant local economy through good accessibility and a quality public realm which provides for inclusive design and universal access for all to the greatest extent possible.
- Promote Climate Action and the decarbonisation of Clondalkin through appropriate design solutions, higher densities closer to existing or planned high-capacity public transport links, where the context allows, and support for the actions on decarbonisation set out in the SDCC Climate Action Plan.
- Provide appropriately phased quality social and community infrastructure at accessible locations within larger scale development, or at locations agreed with the Planning Authority as part of good placemaking to benefit the wider community.
- Ensure that all development uses materials and finishes which are appropriate to their context within the Plan area.



## 8.3 Urban Design – General

### Existing Residential Areas

Clondalkin has experienced several phases of residential development over the twentieth century. These include lower density suburban type housing in different phases, typically three bed with own door and gardens. More recently there has been development of medium-high density residential infill. The different residential character areas are shown in Figure 8.2.

Significant areas for development have not been identified within existing residential areas as part of the LPF. The exception, seen as an opportunity site, is the Dutch Village where principles for development are set out further in this chapter. In all other cases, proposed development will be considered through the planning application process having regard to the relevant standards and objectives set out in the CDP and to the objectives set out within the chapters of this LPF, where they are relevant.

### Objective UD 1 Existing Residential

#### UD1 Objective 1

To assess applications for development within existing residential areas as they arise against the policies and objectives contained within the County Development Plan and the objectives set out within the chapters of this LPF, where they are relevant.

### Commercial / Industrial Areas

The LPF includes areas which are more commercial / industrial in nature. In addition to the commercial central village area covered by the Village Framework below, industrial areas include Oakfield to the east of Ninth Lock Road. The LPF has not proposed any specific objectives for this area other than to identify it as an area which could provide improved east-west movements between the Ninth Lock Framework site and residential areas to the east. As part of this there is the opportunity to improve green infrastructure within the employment area. As the area is zoned Town Centre, there is potential for more mixed use within it. Any applications coming forward will be considered through the normal assessment process having regard to the wider objectives set out in this plan and the CDP.

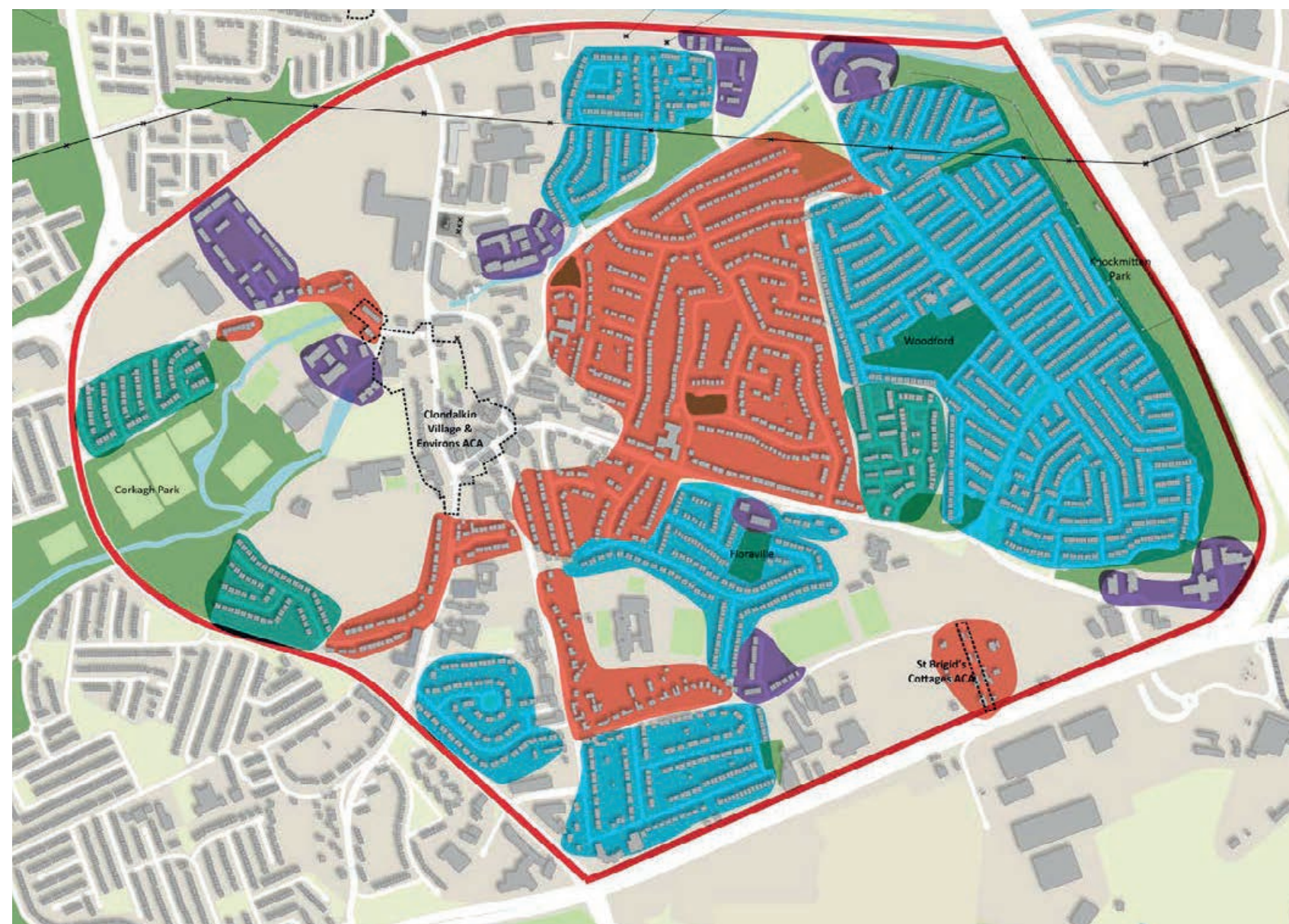
### Objective UD2 Commerical / Industrial Areas

#### UD2 Objective 2

To assess applications for development within the existing industrial estates to the east of Ninth Lock Road, north of the village as they arise against the policies and objectives contained within the County Development Plan and the objectives set out within the chapters of this LPF, where they are relevant.

#### UD2 Objective 2

To improve connections at Oakfield / Elmfield in an east – west and north – south direction where opportunity arises to provide for increased permeability between the residential areas to the east and the Framework site to the west, ensuring that opportunity to improve green infrastructure enhancement and linkages is promoted to the greatest extent possible.



- Mid-century garden city (approx 10-20dph)
- Late-century low density (approx 15-20dph)
- Boom-era housing estates (approx 30-60dph)
- Modern medium-high density infill (100+dph)

**Figure 8.2:** Clondalkin Residential Character Areas.





## 8.4 Urban Structure

Clondalkin continues to be structured around the monastic 'oval' of Tower Road, Orchard Road and Main Street. The oval is connected outwards along the original, six historic roads - Boot Road, New Road, Monastery Road, Watery Lane, Ninth Lock Road and Old Nangor Road.

These six roads can be grouped into three 'spines', intersecting in the Town Centre and described below. These spines are loosely connected by a green loop which wraps around the village and surrounding residential areas adjacent to the road network of M50, N7, Fonthill Road and New Nangor Road.

Chapter 5, Sustainable Movement identifies active travel and wider transport movement within the spines. This section addresses how the framework and opportunity sites, and the village enhancement schemes, identified further on in this chapter are influenced by their location within the urban structure and in turn how the urban structure can be adapted and improved where opportunities arise.

The movement spines and the surrounding loop can be described as follows, with the title colours reflecting the spines on the map:

- 1. Station to Station** – from Clondalkin-Fonthill train station, through the historic centre, onto the Red Cow Luas and Park & Ride.
- 2. Corkagh to Canal** – A green link through the heart of the centre, connecting Corkagh Park, through Clondalkin Park, along the length of the Camac River and onto the Grand Canal Greenway at New Nangor.
- 3. Green Periphery** – A series of large green spaces around the periphery hint at the opportunity to connect these up into a network of safe and green links.
- 4. Tallaght to Grand Canal** – this route follows the hisotric roads of New Road, Boot/Convent Road, Tower Road and Ninth Lock Road. Subject to detailed design, the alignment of the Cycle South Dublin route will be aligned as far as the canal through the village centre. See Sustainable Movement, Chapter 5.

### Urban Structure Strategy

As illustrated in Figure 8.3, the movement spines and the surrounding loop are described in more detail below.

Place specific proposals arising from each are outlined within the relevant Framework and Opportunity site sections and in the village enhancement schemes set out in section 8.6 below.

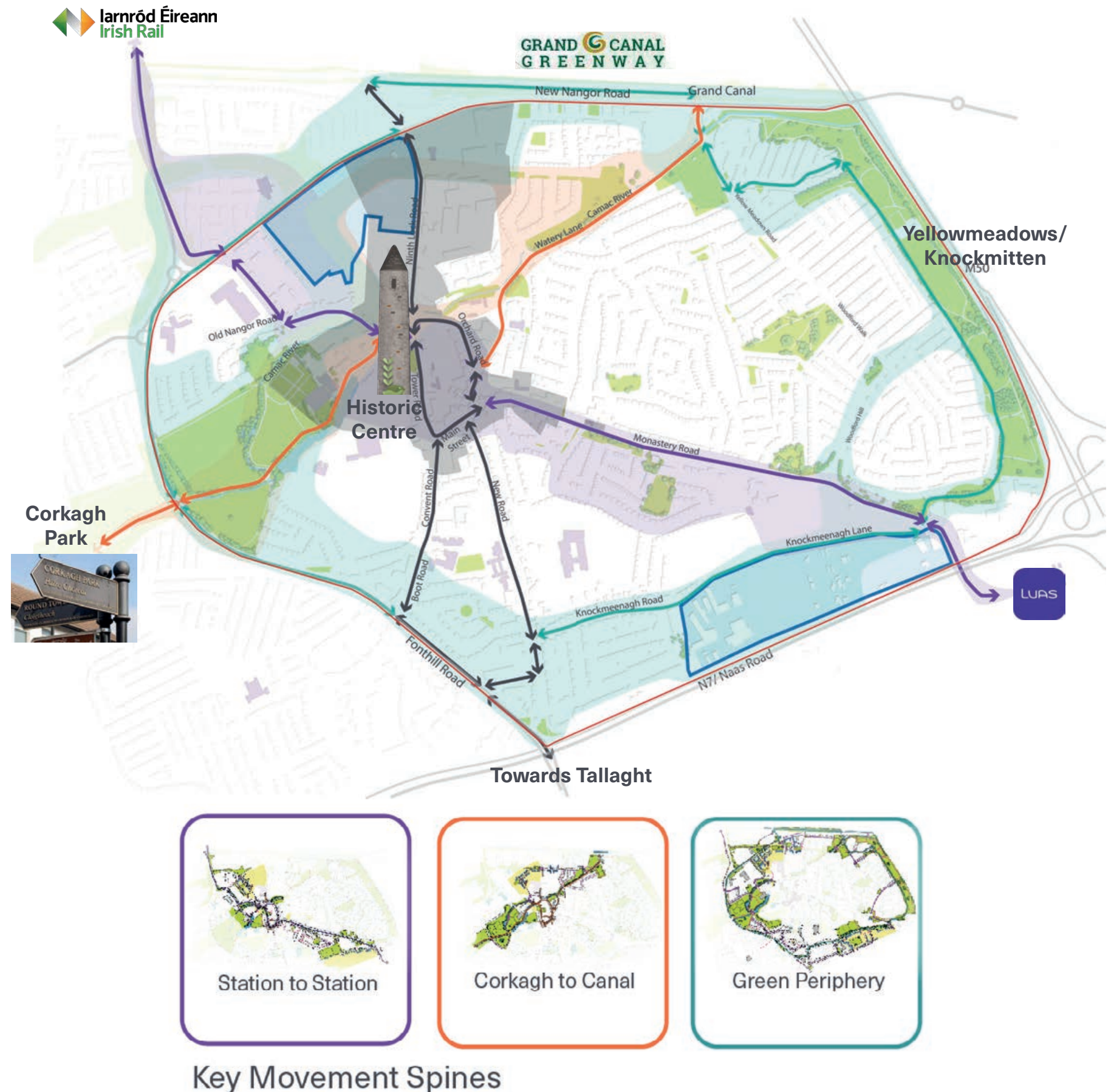


Figure 8.3: Urban Structure Strategy and Key Movement Spines



**1. Station to Station:** from Clondalkin-Fonthill train station, through the historic centre, onto Red Cow Luas and Park & Ride.

The primary route on this spine follows the road network from Fonthill Road North and New Nangor Road, through the Town Centre, and along Monastery Road. This route includes some opportunities for improvement.

The secondary and tertiary routes offer opportunities for quieter routes away from the main roads. These are suited to active travel modes. Some of these are existing and some aspirational. The Ninth Lock Framework site includes opportunities for secondary and tertiary connections.

**Station to Station Objectives:**

- ➔ To improve the pedestrian environment between Fonthill Road North and New Nangor Road.
- ➔ Improve the pedestrian environment and road crossings and connections to the train station from the Ninth Lock Framework site and from Old Nangor Road at the existing pedestrian / cycle lane to the east of Coláiste Chilliain and Gaelscoil Chluain Dolcáin
- ➔ Improve footpaths and cycle facilities at the eastern end of Old Nangor Road and through the village as part of any VES
- ➔ Investigate opportunities for improved pedestrian facilities on the southern side of Monastery Road, in particular from Joe Williams Bridge to the Round Tower GAA lands.
- ➔ Investigate opportunities for an active travel link from Old Nangor Road to Tower Road through Clondalkin Park, providing for a new crossing over the Mill Ponds through lands at Moyle Park.
- ➔ Ensure that opportunities to improve permeability within the village centre and its surrounds are promoted through the Council and engagement with local residents and reflected, where relevant, in any future development. See Chapter 5.
- ➔ Investigate opportunities to enhance the disused water tower structure as a way-finding device, signalling the centre.
- ➔ Safeguard the delivery of an active travel link across the SIAC quarry, towards Knockmeenagh Lane.

These objectives are reflected further in the relevant framework sites and VES.



**Figure 8.4:** Station to Station Spine





**2. Corkagh to Canal:** A green link through the heart of the centre, connecting Corkagh Park, through Clondalkin Park, along the length of the Camac River and onto the Grand Canal Greenway at New Nangor Road.

The primary route on this spine is partially within the open spaces of Corkagh Park and Clondalkin Park, before coming out onto the roads within the village centre and at Watery Lane. This route includes significant opportunities for improvement as it is fragmented, difficult to identify in places and indirect particularly though the centre.

It is intended that this spine will incorporate the Corkagh Park to Grand Canal Cycle South Dublin scheme.

The secondary and tertiary routes offer alternative options, for quieter routes away from the main movement spine. These are not immediately achievable, because of significant barriers to movement along the Camac corridor, through the Ninth Lock Framework Site and through Oakfield into Riversdale and on into Mayfield.

Significantly, where this spine deviates from the Camac River channel the legibility of the route becomes fragmented, and wayfinding is reduced. Legibility of this spine could be improved through providing new connections set back from the river, having regard to the riparian corridor and using materials and planting at ground level to signal the route where it continues to deviate from the river.

**Corkagh to Canal Spine Objectives:**

- ➔ Investigate opportunities to signal the parks on either side of the Fonthill Road, whether through additional greening, opening up views and improving the access points.
- ➔ Maintain the newly planted mini-woodland within Clondalkin Park as an enhancement to green infrastructure.
- ➔ Ensure that opportunity sites on Old Nangor Road, and the VES, is designed in such a way as to form part of a way-finding strategy towards Clondalkin Park, with materials, planting and frontages being used as tools to signal the proximity of the park.
- ➔ Improve wayfinding between Sally Lane and the Civic Space opposite the Mill Shopping Centre, including safeguarding the potential for an alternative and more direct active travel route through the Mill Shopping Centre from the Ninth Lock Framework site.
- ➔ Pursue opportunities to create an active travel off-road link between Orchard Road/Lane and Watery Lane.
- ➔ Investigate a spatial reorganisation of footpaths,

- carriageway and open space between Áras Chrónáin and the entrance to Riversdale to improve the pedestrian/ cyclist experience and signal the route towards the village centre.
- ➔ Promote the Cycle South Dublin scheme along this spine, facilitating off-road routes where feasible.
  - ➔ Ensure that active travel routes at Knockmitten are connected to the new Bus Connects access points to the Grand Canal

- ➔ Promote an active travel link from Clondalkin Park to Old Nangor Road, on the south side of the Camac River, ensuring that proposed development along this route facilitates such a linkage.
- ➔ Investigate opportunities for improved public access to and visibility of the Camac River at lands to the north of Watery Lane at the junction with Woodford Walk.

These objectives are reflected further in the relevant framework sites.



**Figure 8.5:** Corkagh to Canal Spine



### 3. Green Periphery: A series of large green spaces around the periphery hint at the opportunity to connect these up into a network of safe and green links.

This presents the opportunity for an informal green loop around the LPF boundary, connecting large parks of Clondalkin and Knockmitten, through smaller green spaces and opportunities for greening.

The primary green loop can be generally viewed as two parts:

- incidental green spaces along Fonthill Road and New Nangor Road and footpaths adjacent to the traffic, and
- moderate quality routes with limited overlooking through Knockmitten Park and along Knockmeenagh.

The primary routes are supported by an informal secondary and tertiary network, mostly interspersed through residential streets, with gaps in connectivity, kissing gates, kerbs, partially hidden turns and unsignalled road crossings along the way.

This presents opportunities for improvements to both green infrastructure (See Chapter 4) and to movement through better active travel links and connections to the other two movement spines.

Green Loop Spine Objectives:

- Investigate options for active travel links between Newlands Road to Newlands Cross and the intensification of planting to improve the environment, where feasible.
- Promote increased planting at the green space at Newlands Garden Centre and from the southern end of Fonthill Road from Newlands Cross to St. Brigid's Well as part of a pilot for grey to green surface water proposals, ensuring that the setting of St. Brigid's Well is protected.
- Safeguard the historic connection from Fonthill Road to St. John's Road as an important secondary link from the village centre to Sacred Heart National School.
- Promote the re-opening of the historic entrance to Moyle Park College from St. John's Wood as an alternative route for active travel during school hours. Further to investigate the continuation of this link into Clondalkin Park by way of the Sandy Hole and lands to the north of Moyle Park College building.
- Investigate the opening of the gate from Cherrywood Avenue to Old Nangor Road either during school hours

or permanently, to support active travel routes to school through Clondalkin Park.

- Support opportunities for improved road crossing, planting, lighting and overlooking along the length of the existing pedestrian link between New Nangor Road and Old Nangor Road, recognising it as an important link to three schools – Coláiste Chillian, Gaelscoil Chluain Dolcáin and the relocated Gaelscoil na Camóige.
- Promote a pedestrian and green connection within any buffer zones under the pylons, proving for east-west connections across the Ninth Lock Road.
- Consider how the Ninth Lock Cottage and adjacent green space can support wayfinding between the Grand Canal and the village centre.

- Where opportunity arises, safeguard the delivery of active travel links between the Oakfield industrial area as far as Woodford Walk to the east and New Nangor Road to the north.
- Investigate the feasibility of a pedestrian crossing from Woodford Parade to help access the existing active travel link through Woodford Crescent to Knockmitten Park.
- Support improved road crossings between Knockmitten Park and Knockmeenagh Lane as an important connection in active travel.

These objectives are reflected further in the relevant framework sites and in Chapters 4 and 5.



Figure 8.6: Green Loop Spine





4. **Tallaght to Clondalkin - Grand Canal:** In the Cycle South Dublin scheme an indicative route follows the historic roads of Boot/Convent Road, Tower Road and Ninth Lock Road. The route will predominantly follow the existing road network where space for additional interventions is limited. Due to the constraints along Boot / Convent Road the Local Transport Plan indicates a more realistic route for the active travel Cycle South Dublin scheme is along New Road. The route shown below is indicative and detailed design will be subject to public consultation

Tallaght to Clondalkin - Grand Canal Objectives:

- ➔ Support the Cycle South Dublin active travel Tallaght to Grand Canal scheme, subject to its detailed design and further consultation to be carried out as part of the Part 8 process.
- ➔ To ensure that the routing of the scheme through the village centre is designed as an integrated part of the Village Enhancement Schemes.
- ➔ Promote a way-finding strategy from the Grand Canal to the village centre, ensuring the use of consistent signage and materials.

8.5 Frameworks and Opportunity Sites

This section provides parameters and objectives which will guide the future growth and development of Clondalkin for the Village Framework Area and associated Mini- Frameworks, Ninth Lock Road Framework, Knockmeenagh Framework, the Village Enhancement Schemes (VES) and identified Opportunity Sites

8.5.1 Village Centre Framework Site

This framework examines the historic and mixed-use village centre of Clondalkin. While not being a single potential development area the village centre has some potential for future development and presents opportunities for improvements which will contribute to the consolidation of the existing historic urban form, improve permeability and movement across the LPF area and enable public realm and placemaking interventions which will serve the entire LPF area. The village centre is outlined here as the area within the bounds of Oakfield to the north, St. Killian’s scout den on Watery Lane, the Carnegie Library on Monastery Road, the Moyle Park gate lodge on Boot Road and the Camac bridge on Old Nangor Road. This area includes the designated Clondalkin Village ACA. See Figure 8.9.



Figure 8.5: Corkagh to Canal Spine

The existing urban form of the Village Centre is mixed in terms of typologies, building heights and scale of buildings.

The Village Centre Framework area presents a unique opportunity to integrate proposals at the strategic and local level in more detail allowing the strategy to address the fine grain built form of the Village Centre. Similarly, several locations within the village centre are of sufficient importance, such as the historic junctions and streets of the village centre that they are addressed in the Village Enhancement Scheme (Section 8.6). These areas are presented by means of indicative layouts demonstrating site-specific public realm and open space interventions within the urban area.

The key Urban Design Principles for future development within the Village Framework Area are set out below;

Table 8.1: Urban Design Principles for the Village Framework Site

Urban Design Principles for the Village Centre Framework Area
Improve the main north- south route through Clondalkin along the Ninth Lock Road, through high quality design and placemaking, including through the design of the proposed village enhancement scheme, seeking to create solutions to blank frontages where they exist.
Ensure proposed development responds to the surrounding context with transitions provided where necessary, with taller buildings located along key movement corridors, gateways and nodes. In this regard, proposed development must demonstrate how it has had regard to context as set out in SDCC Building Height Guidelines, Appendix 10 to the County Development Plan.
Ensure that proposals and their built form integrate in an appropriate manner with the prevailing character and context of Clondalkin Village and the ACA area. See Conservation Chapter for detail.



### Urban Design Principles for the Village Centre Framework Area

Ensure building edges engage with the street creating a smooth transition between street and buildings and actively promote well-designed buildings and public spaces that provide for active frontages and 'live' edges that feel safe, secure and attractive for all to use.

Prioritise pedestrian movements and improve pedestrian mobility within the village whilst calming traffic.

Enhance biodiverse soft landscaping within the village and seek to create connections and new stepping stones enhancing the existing GI network.

Encourage new development to provide niches of open space or urban pocket parks as buffers to transition between the historic village and new development.

Improve wayfinding across the village centre and establish way finding parameters where new connections are established or existing improved arising from the provisions of this chapter. (See CBH8 Objective 1.

All development must also comply with the provisions of the County Development Plan. The County Development Plan 2022-2028 sets out the following objective:

#### EDE11 Objective 4:

To protect the historic village core of Clondalkin, recognising the role it has in placemaking and the of the town to retailing and other functions.

The current County Development Plan also includes the following Specific Local Objective relating to Clondalkin Village Centre:

#### NCBH21 SLO 1:

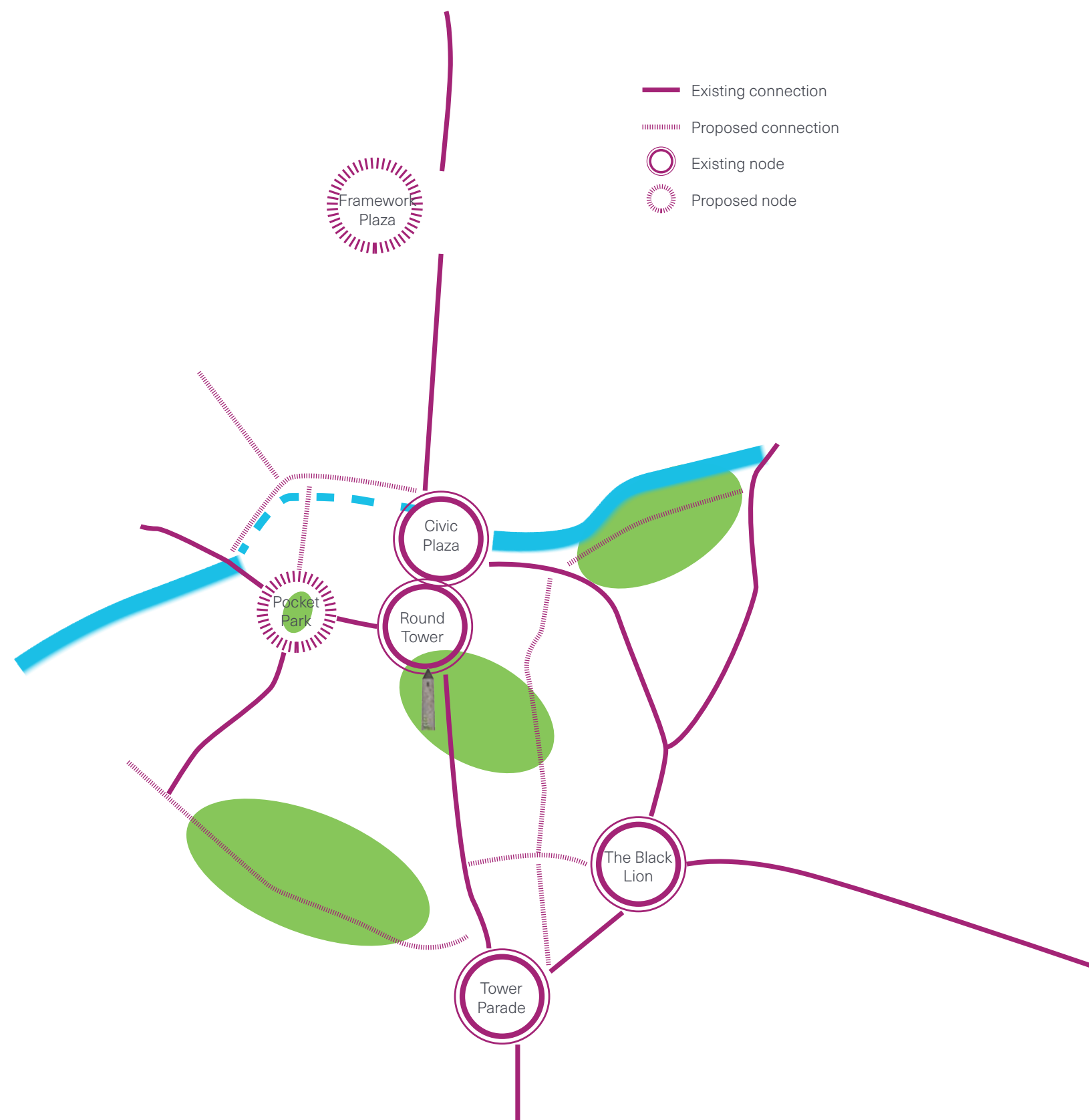
To protect and maintain the remaining old stone walls of Clondalkin.

The focus of the Village Centre Framework is to provide guidance around future public realm and GI measures, pedestrian accessibility, legibility and wayfinding within the area and the protection and enhancement of the cultural, built and natural heritage features, ensuring a balanced approach to the introduction of new build proposals. See Figure 8.9 along with associated text.

### Nodal Points

The village centre also has multiple key existing nodal points. These are indicated on Figure 8.8 as the Round Tower, Tower Parade, Clondalkin Leisure Centre, the Mill Shopping Centre and the Black Lion.

The 20th century realignment of the junction at Orchard Road and Ninth Lock Road has created an additional Civic Plaza which also serves as a key nodal point. This strategy proposes the addition of two more nodes at the Ninth Lock Framework Site in the form of a plaza and the junction of Mill Lane and Old Nangor Road as part of the VES signalling the connection to Clondalkin Park. The VES schemes later in this chapter address opportunities affecting some of the primary nodal points in the village. Existing and proposed permeability and GI links further connect and reinforce these nodal points



**Figure 8.8:** Existing and proposed nodal points within the village and their interconnections.



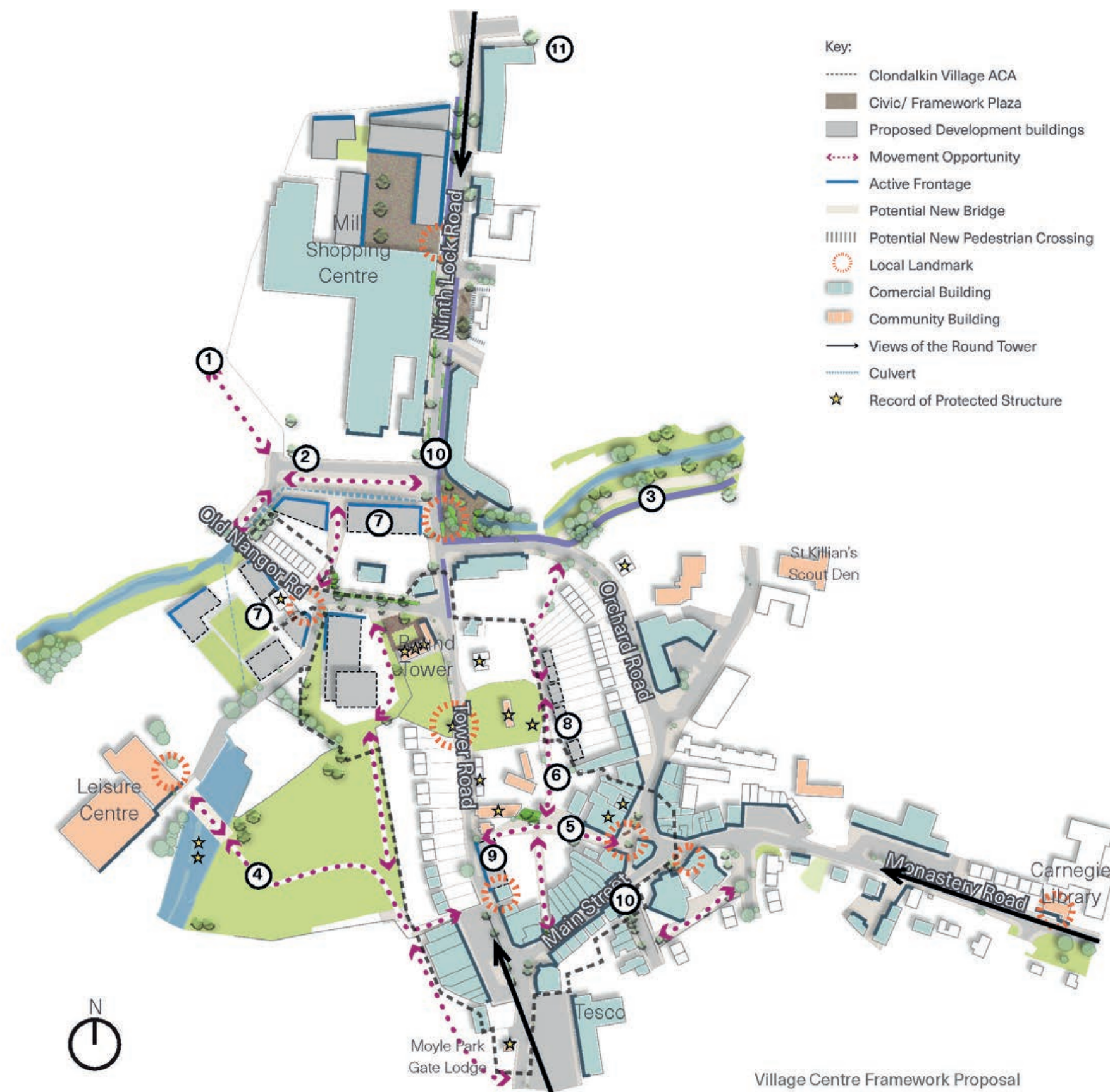


Figure 8.9: Village Centre Framework Area

### Opportunities within the Village Centre

Figure 8.9 indicates through the numbering on the map and associated commentary, different areas where key connections in the village centre could be improved, alongside development and public realm opportunities. In terms of linking different areas within the village, they are identified in the context of urban design and the benefit of good connections. It is recognised that many of the lands identified are in private ownership and will only come forward through new development and / or as part of agreements with the landowner.

### Opportunities for Greater Connectivity:

1. Ninth Lock Framework Site safeguards a connection on the potential alignment of a high-capacity transport route, from the Mill car park towards the open space at Dunawley, as identified on the County Development Plan. This is the most direct connection to Clondalkin-Fonthill Train Station and Clonburris National School from the village. This same route presents an opportunity to create green linkages within the Framework Site itself, southward to the village and Mill Shopping Centre Site (connecting with GI elements of any re-development of this site). Potential GI connections eastward also through the Oakfield development area.
2. Camac alignment connection: There is a long-term opportunity locally for re-development of the surface parking area at the Mill Centre with town centre appropriate blocks. This potential should be safeguarded, with the future road adjacent to the Camac culvert alignment re-establishing the original route of the Camac and enabling a modern permeability route. This allows for the potential future de-culverting of the river in tandem with development. This presents an opportunity to change the entire character of the town centre in time. The interaction between any proposed active travel route and the protection of a green link re-establishing this historic biodiversity and GI route along the route of the Camac needs to be carefully considered. The delivery of both will need a considered design approach.
3. Orchard Road to Watery Lane along the route of the Camac: There is a strong desire line identified by the Cycle South Dublin alignment study between Orchard Road and Watery Lane generally following the route of the Camac. Here the existing riparian corridor along the Camac is a huge asset. The options for delivery of this active travel route while protecting the existing riparian corridor should be considered further.
4. Village Centre Open Space: The village centre would benefit strongly from improved access to Clondalkin Park, and onwards towards Corkagh Park. This connection would provide both permeability and GI potential. As a link, this is the most direct route towards the Fonthill Road by way of the Leisure Centre and Clondalkin Park (See Corkagh to Canal Spine). Furthermore, the north-south link connecting this open space to Old Nangor Road helps improve permeability between Clondalkin Park and the village centre, while also offering a quieter, more scenic, and pleasant alternative route.





5. East-west through the 'oval': There is also long-term potential for a strategic east-west link through the central 'oval' of the village from the Black Lion junction through to Tower Road or southward to the Main Street via the car parking at the 'Steering Wheel'. This is a historic link with the potential as a secondary route for pedestrians, away from the trafficked street. The Black Lion frontages already provide some degree of overlooking and activity on this route.
6. North-south through the 'oval': With the potential for the east-west route connection through the 'oval' of the village, there is further opportunity to be investigated moving south towards Main Street and north along the edge of St John's Churchyard. There is an existing Mews Lane to the rear of Orchard Road, understood to be in private ownership. If this link can be brought all the way through the central 'oval' these mews buildings could come into use as live-work units, artists' studios or similar small scale mixed-use appropriate to a historic village centre. Similarly, there is potential to link to the east-west route connection through the 'oval' with Main Street.

### Opportunities for Development and Public Realm Improvements:

Figure 8.9 also indicates potential development and public realm opportunities within the village centre.

7. Opportunities for infill development at Old Nangor Road and Mill Centre Car Park. These opportunities are explored in the Mini-Frameworks piece of this chapter (Section 8.5.2).
8. As noted above, if the existing mews lane to the rear of Orchard Road can be connected up to the east-west link then there is potential for mews infill on this lane.
9. Tower Shopping Centre: The east-west link may be deliverable with the current alignment of Tower shopping centre, there is currently a narrow opening to the northern edge, adjacent to St John's National School. In the longer-term it would be preferable to see this shopping parade redeveloped with frontage to Tower Road. This would support better streetscape and enclosure on Tower Road. It would also support reduced traffic movements on Tower Road. Parking could be relocated to the rear as necessary and appropriate.
10. Village Enhancement Scheme (VES): The Ninth Lock Road and Old Nangor Road VES and Main Street VES offer opportunities to redistribute space towards pedestrians, cyclists, planting and activation while maintaining vehicular access. This can be achieved through public realm and streetscape improvements such as tree planting, widening footpaths, creating active travel routes, and implementing nature-based solutions, including SuDS. Further details on VES can be found in Section 8.6.
11. Oakfield: There is a long-term opportunity for infill and some mixed-use development at Oakfield. The area is designated a Town Centre (TC) zoning and offers a potential to integrate with Clondalkin village centre. Furthermore, the area offers potential links from key existing and proposed nodes on the Ninth Lock Road with adjoining residential areas to the east.

### Objectives for the Village Centre Framework Area

The following objectives under key themes build on relevant objectives set out in previous chapters.

#### VF 1: Movement and Permeability Objectives

##### Movement and Permeability

Ensure best practice urban design principles are incorporated in all movement related aspects of proposed development or schemes within the village area.

Support the delivery of the cycle and pedestrian permeability routes in the village centre outlined in Figure 8.9 above and Figure 5.9 of Sustainable Movement, Chapter 5, ensuring the development of any such routes is subject to public consultation and that any necessary landowner agreements are in place.

##### Active Travel Links

- Route through Ninth Lock Framework site linking Clondalkin Village with residential areas to the north and to Fonthill Railway Station.
- Route linking Orchard Road and Watery Lane (active travel).
- Tower Road through Moyle Park lands and Clondalkin Leisure Centre towards Old Nangor Road and towards Fonthill Road.
- Route southward from Ninth Lock Framework and Mill Centre car park lands to Old Nangor Road and the Snooker Hall site southward through Moyle Park and Clondalkin/Corkagh Park
- Links from Ninth Lock Framework site across Ninth Lock Road to Oakfield with potential further linkages to the east.
- From New Nangor through Lidl site and southward through Oakfield.
- Potential links through the 'Village Centre' oval as shown in Figure 8.9.

#### VF 2: Green and Blue Infrastructure Objectives

##### Green and Blue Infrastructure / Surface Water Management

Ensure compliance with the provisions of chapter 4 of the South Dublin County Development Plan 2022-2028 and with the objectives set out for Clondalkin in Chapter 4: Green Infrastructure of this document.

Ensure that all proposed development incorporates SuDS in accordance with the SDCC SuDS Guidance.

##### Opportunities

- New development/re-development within the Village Framework Area shall address any gaps identified in the GI assessment in Chapter 4 of this document.
- Support the delivery of Village Enhancement or similar schemes and encourage the use of SuDS and native planting within the village centre where possible. Limit the removal of trees and where feasible support appropriate re-planting.
- Implement the Green Space Factor requirements of the CDP delivering an increase in net GI value
- Provide for native tree and ecologically friendly planting.

#### VF 2: Green and Blue Infrastructure Objectives (continued)

##### Green Links

- Demonstrate how green links that intersect with the village centre (GI Stepping Stones) set out in detail in chapter 4 of this document under GI3 are incorporated into and will be delivered through any future development/re-development within the Village Centre.

##### Camac River Riparian Corridor

- Require hydromorphological assessments where development lands are within the Camac riparian corridor and ensure that proposed development is sufficiently informed by their findings.
- Require development to include measures to protect and where possible extend riparian corridors.
- Ensure that where active travel routes are adjacent to the riparian corridor that they are sufficiently set back from the river to ensure the integrity of the corridor. In the absence of sufficient setback not being feasible, ensure that mitigatory planning or other measures are introduced to augment the corridor.
- Encourage the de-culverting of the Camac to support the integrity of the Camac riparian corridor, increase opportunities for biodiversity and significantly improve placemaking opportunities for the wider village.
- Ensure that in the shorter term, any re-development of the Mill Shopping Centre car park, or plaza outside the civic offices, includes planting and SuDS at ground level following the route of the Camac culvert.

### Conservation and Built Heritage

As set out in Chapter 7 the historic urban settlement of Clondalkin Village can be traced back to the monastic site founded by St. Cronan Mochua in the 7th century. Features originating from this period remain an intrinsic feature of the present townscape. Part of the centre of the village is designated as an Architectural Conservation Area and lies within an area of Archaeological Potential. The village centre also includes a number of recorded monuments (RPS), as set out in the accompanying Conservation Plan to this LPF and for which objectives are set out in Chapter 7 of this document.

Other conservation and built heritage considerations within the village centre include the historic walls, other structures of interest, materials, boundary treatments, building typologies, and architectural detailing. Further detail and objectives on these elements is detailed in Chapter 7 of this document. The LPF is also accompanied by an ACA appraisal entitled '*Clondalkin Village Architectural Conservation Area Appraisal and Recommended Safeguarding Policies*'. The detail of these documents should be referred to where any development impacting any of the above is being considered.





### VF 3: Conservation and Built Heritage Objectives

#### Conservation and Built Heritage

Development potentially impacting on the built heritage elements of Clondalkin Village set out in Chapter 7 of this document shall comply with the relevant objectives set out in that chapter. The following directly apply to the village centre but are not exhaustive:

#### Architectural Conservation Areas

- Applications within and contiguous to the ACA to be accompanied by an Architectural Impact Assessment.
- Demolition of structures which are deemed by the Council to positively contribute to the ACA is prohibited.
- Development to comply with recommendations set out in 'Clondalkin Village Architectural Conservation Area Appraisal and Recommended Safeguarding Policies' which accompanies the LPP.
- Encourage adaptive re-use of historic buildings
- Ensure that new development respects the proportions and scale of the existing urban structure and modest vernacular building designs.

#### New Development

Support infill and backland development which is appropriate in character and scale to Clondalkin's historic village centre

#### Views and Vistas

- Incorporate incidental vistas of heritage features, such as the Round Tower and church spires and convent.  
See also CBH 6 Objective 1.

#### Visual Clutter

- Reduce visual clutter from signage, street furniture and road markings across the village including the ACA.
- Ensure consideration at the early stage of development to be given to location and appearance of services where they interact with public realm.
- Work to improve the appearance of shop fronts and promote SDCC's Shop Front Grant Scheme and Shop Front Design Guide to enhance visual cohesion in the village.

#### Front Gardens

- Protect and enhance the sylvan nature of gardens fronting onto the village including along Orchard Road, Tower Road and Mill Lane, resisting the creation of additional car parking in front gardens, where it would further erode the character of the village.

#### Historic Streets

- Protect and enhance the historic character of streets such as Mill Lane which feature vernacular proportions, walled boundaries and general rural character in its framing of the Old RIC Barracks and access to Clondalkin Park.
- Protect the unique setting for structures of historic significance along Monastery Road and New Road both feature historic buildings and walled structures.

### Historic Walls

Objectives for the historic walls within and outside the village areas are set out in Section 7.4 of Chapter 7. They are repeated here for ease of reference.

### VF 4: Historic Walls Objectives

#### Historic Walls

- To protect and preserve the identified historic limestone walls of Clondalkin whether located within or outside of designated ACA areas (see Figure 8.10 - Historic Walls identified in Clondalkin Village and environs) and ensure that any future development proposals will have due regard for the historic nature and importance of these walls.

#### Objectives from Chapter 7

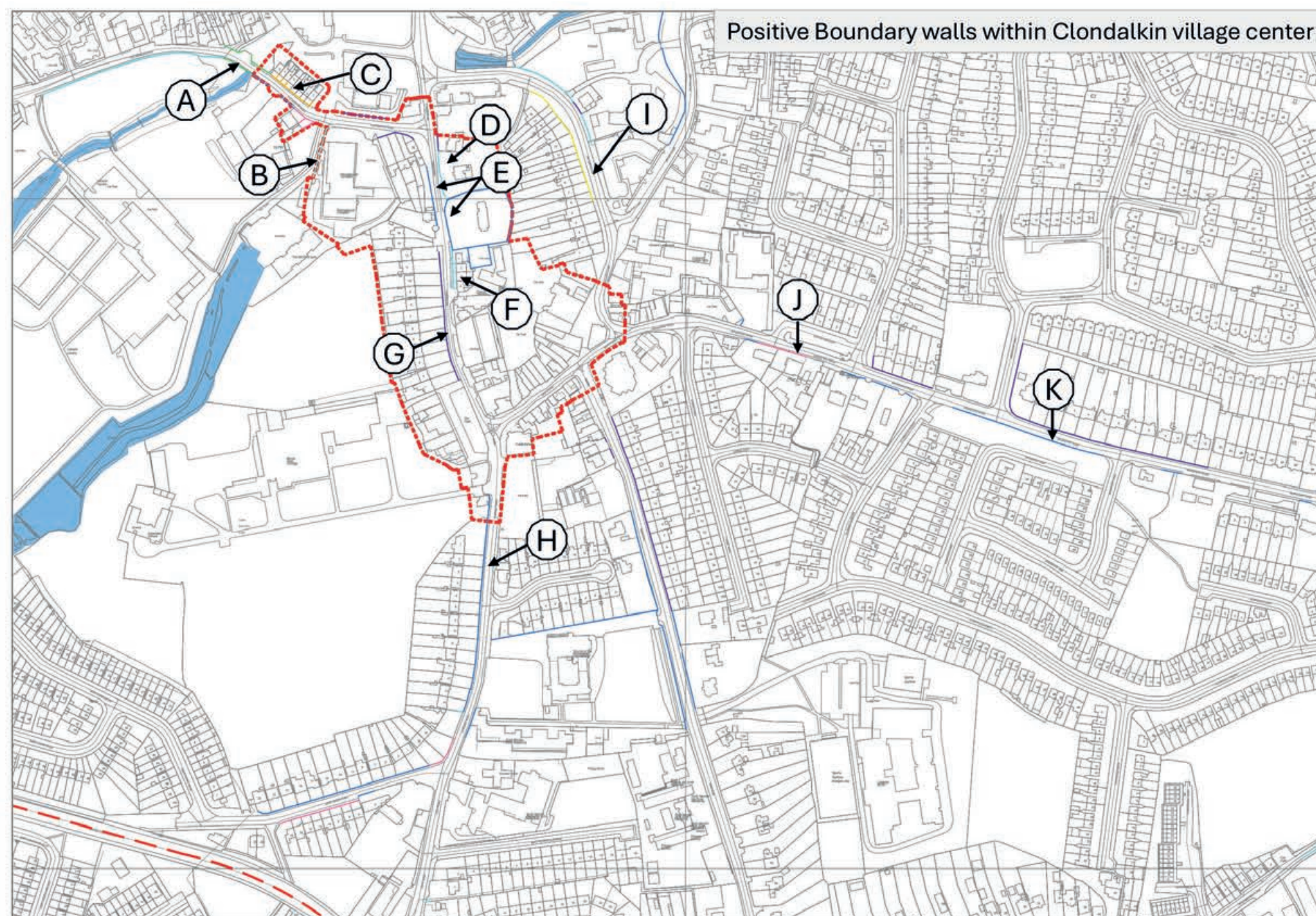
- To ensure protection of historic gate piers, gates and entrances, safeguarding these important features so that they continue to enrich the quality of the public realm CBH5 Objective1)
- To require new boundary treatments or reconstruction of boundaries to reflect where appropriate, the composition and materials of traditional boundary / entrance treatments (CBH5 Objective 2)
- To ensure that where development is proposed which includes a boundary or other wall, and where it is not known whether this wall that an examination, including a report, of the wall is undertaken by a qualified professional prior to any proposal for development. Where the wall is identified in this way as historic it shall be protected and designed in to any development proposal (CBH5 Objective 4)

### VF 4: Historic Walls Objectives

- To ensure that sufficient set back is provided where development is proposed adjacent to historic walls, recognising that their foundations may be minimal and / or they may be vulnerable to development (CBH5 Objective 5)
- To maintain historic walls in accordance with best practice, repairs should be undertaken by a competent craftsman using traditional methods and materials, where possible. If this is not possible a clear rationale should be set out as to why, alongside the proposed alternative solution. The proposed methodology for repair shall be submitted to the planning authority for agreement prior to the repairs or ground works / development being carried out (CBH5 Objective 6)
- To encourage new boundary walls to be low walls finished with roughcast render with solid limestone cappings, as generally preferable to the application of limestone cladding or veneers. (CBH5 Objective 7)
- To require any proposed development along Orchard Road which may impact on any existing walls within or enclosing the properties fronting the road to be accompanied by a report from a suitably qualified person indicating the location of any upstanding masonry within existing walls which may contain remnants of the old monastic boundary of Clondalkin (fosse) Recorded Monument DUO17-041001. Any identified remains shall be recorded and protected in accordance with best conservation practice (CBH5 Objective 8)







#### LEGEND

##### Wall Type 1

Rubble limestone with rounded or flat copings.

##### Wall Type 2A/2B

Rubble limestone wall with 'cock and hen' copings, roughcast render or weathered so that limestone masonry is fully exposed.

##### Wall Type 3

19<sup>th</sup> C Bridge parapet with rock faced copings.

##### Wall Type 4

Low brick / rubble wall with render finish, rounded coping.

##### Wall Type 5

Positive boundary, mid 20<sup>th</sup> century and contemporary, various styles. Note: earlier fabric may be concealed or reused within.

Wrought iron railing

Plinth following line of fosse

ACA Boundary

LAP Boundary



Bridge parapet walls, in roughly coursed limestone blocks with rock-faced copings.

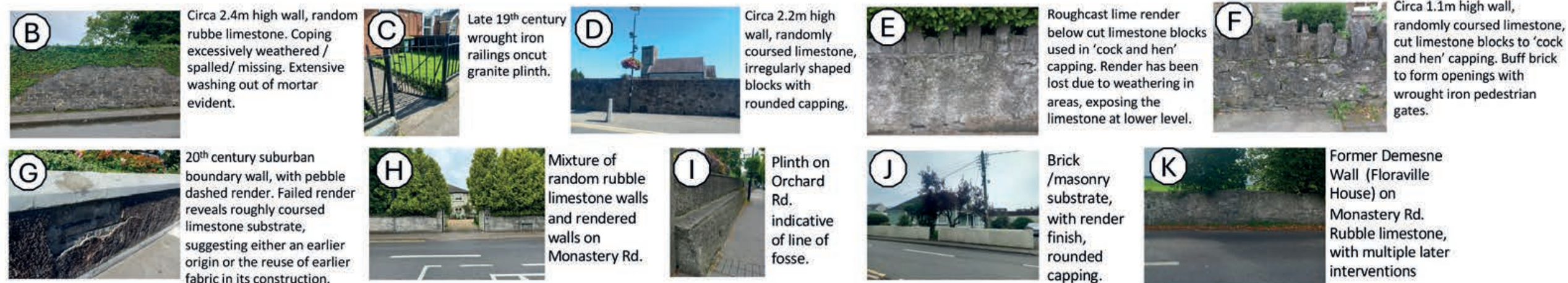


Figure 8.10: Historic walls identified in Clondalkin Village and environs





### 8.5.2 Mini Framework Sites

Three Mini Framework Sites have been identified within the Village Centre Framework Area and they are expanded on below.

#### Mill Centre Car Park

The Camac River enters a culvert at Old Nangor Road and re-emerges on Orchard Road. In addressing identified gaps in green infrastructure, the Council support the de-culverting of the Camac as part of any future redevelopment of the car park at the Mill Shopping Centre. Should redevelopment of this site occur, consideration should be given in the first instance to de-culverting as part of development or where de-culverting is not feasible at the time, to a street alignment within the development that allows for long term de-culverting.

Development at this site should make a positive contribution to the Village Centre by improving street frontage and enclosure on Old Nangor Road and Ninth Lock Road.

Together, these actions would prove positive for the village, aiding legibility and improving the human scale of the centre. Future reimagining

of the civic offices and space around them would present an opportunity for complementarity between the spaces.

Any future development of this area should address the following:

**Mill Centre Car Park – Parameters for Development**

**Parameters for Development**

- Ensure that any development improves street frontage and enclosure on Old Nangor Road and has due regard to its historic setting at this location
- Encourage the opportunity of any redevelopment to review the current shopping centre and potential improvements to its presentation to Ninth Lock Road in particular.
- Rationalise car parking across the site to encourage improved access/parking and promote more efficient uses on this site.
- Deliver improvements to the environment around the Mill Shopping Centre including placemaking improvements to the frontage along the Ninth Lock Road (See CHE17 Objective 2)

**Mill Centre Car Park – Parameters for Development**

- Require landowner/developers to engage proactively to review potential access from Ninth Lock Road, both for the Mill Shopping Centre and the Ninth Lock Framework Site, to ensure safe and efficient movement of pedestrians and cyclists, and separately of private vehicles and service/delivery vehicles, thereby improving links to and delivering positive impacts to the village centre.
- Promote the rationalisation of delivery, servicing and loading arrangements for businesses in the shopping centre through potential new access from New Nangor Road requiring constructive engagement with the adjoining landowners of the Ninth Lock Framework site and joint delivery / agreement.
- Any future development shall prioritise the de-culverting of the River Camac through the site or where this is not feasible in the short term, creating a GI soft landscaped and active travel route along the route of the culvert. This key element linking east and south to the VES Schemes and to Clondalkin Park
- Require the delivery of active travel routes linking to the Ninth Lock Framework site and south to Mill Lane.

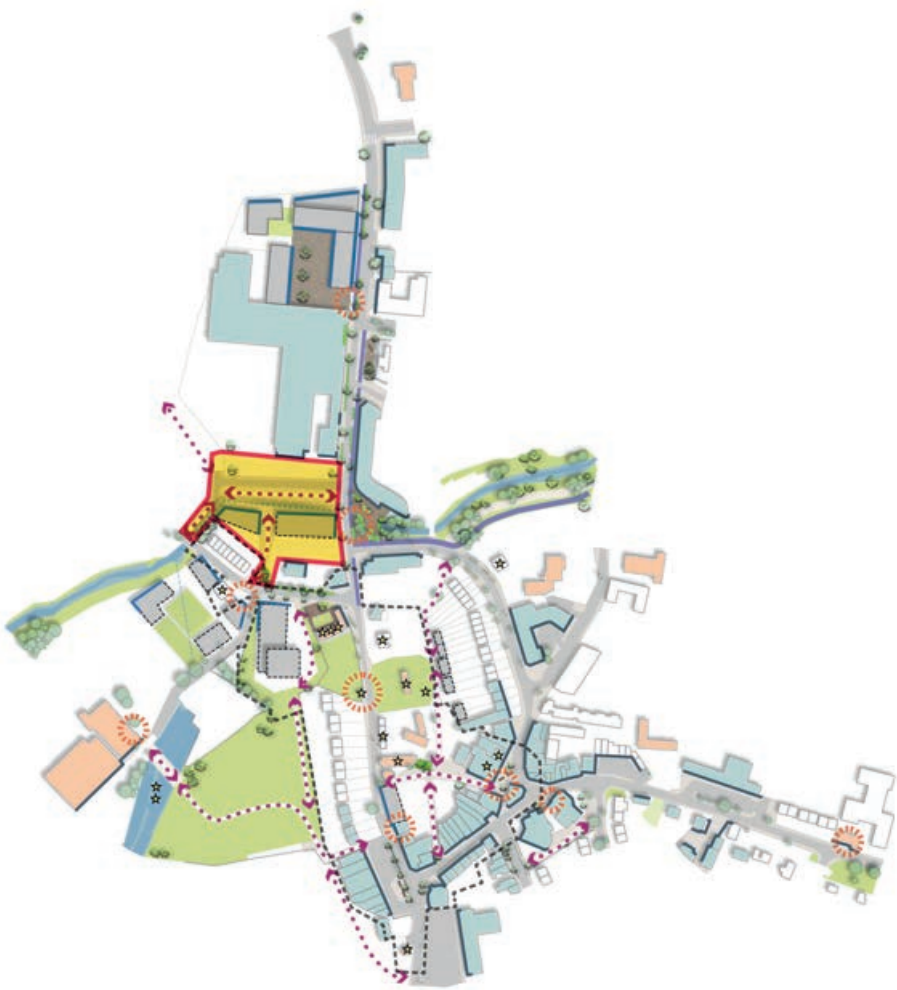


Figure 8.11: Mill Centre Car Park Location



Figure 8.12: Mill Centre Car Park Interventions



### Old Nangor Road Infill Site

The Old Nangor Road, from the Round Tower Visitor Centre to the bridge over the Camac River, is an important piece of the original streetscape of Clondalkin. The overall legibility of the streetscape here is poor resulting in Leinster Terrace and the Old RIC Barracks (Riverside) feeling disjointed from the village. There are opportunities to enhance GI stepping stones and connecting to GI on lands adjoining. There is also long-term potential for the integrated development of lands to the rear of the Old Nangor Road and surrounding the Old RIC Barracks potentially delivering a stronger street frontage but there are significant constraints on these sites relating to flood risk and the fact that the Camac culvert traverses these lands severely limiting their potential re-development.

**Note:** Due to uncertainty around flood extents the provisions of this section are conditional on the completion of the River Camac Flood Alleviation Scheme (FAS). Until such time any development is premature (See the Strategic Flood Risk Assessment accompanying this report).



Fig 8.13: Old Nangor Road Mini Framework Location

### Objective VF 5 – Old Nangor Road Flood Risk

Ensure that no new development takes place within the flood plain of the Camac River at the Old Nangor Road until such time as the measures required as part of the Camac Flood Alleviation Scheme are known (See SFRA accompanying this LPF).

Any future development of this area should address the following parameters:

### Old Nangor Road – Parameters for Development

#### Parameters for Development

- Any future development in this area shall comply with the objectives relating to Built Heritage and Conservation and Clondalkin Village ACA as set out in Section 8.5 of this chapter and Chapter 7 of this LPF.
- Repair the building line and frontage along the full length taking the RIC Barracks as a guide.
- Ensure the integration of the historic features: RIC Barracks, gate pillar, Leinster Terrace where relevant to new development.
- Active or residential frontages should provide passive surveillance of the pedestrian routes on all paths: Camac River, Sally Park Lane, Old Nangor Road.
- Provide strong corner buildings and planting on each side of Sally Park Lane where not conflicting with built heritage objectives, to help wayfind the primary pedestrian entrance to Clondalkin Park.



Figure 8.14: Old Nangor Road Infill Sites Interventions

### Old Nangor Road – Parameters for Development

- New development to integrate with public realm measures, tree planting and SuDs measures, bringing public realm (including any proposed VES Scheme) and tree planting in the town along the length of this stretch of road to support visual cohesion, where feasible.
- Prioritise through any new development coming forward along the Old Nangor Road the delivery of a quality GI connection to Clondalkin Park, allowing for connections through a potential future route through the Moyle Park lands. Any future development of Moyle Park lands to incorporate GI connections across and through the site providing connections between Old Nangor Road and Clondalkin and Corkagh Park and to the open space at the Round Tower visitor centre to the east, subject to future consents.
- All new development shall comply with the Green Space Factor requirement of the CDP.
- All objectives relating to the protection of existing riparian corridors shall be complied with.
- Require the delivery of active travel / permeability routes through this area.
- Proposed development within the Camac River Flood Alleviation Scheme (FAS) catchment is premature until such time as the FAS has been finalised.



Figure 8.15: Artists Impression Old Nangor Road





### 8.5.3 Opportunity Sites

#### Adaptive Reuse of the Old RIC Barracks

The protected structure of Riverside located on the Old Nangor Road (see Figure 8.16) is a five-bay, two-storey house with small gardens to the front and back from circa 1820. The building has been vacant for several years and forms an important part of the streetscape and western boundary of Clondalkin. The re-use of the building as residential could be considered. Similarly, consideration could be given to the adaptive reuse of the building for non-residential uses such as community/ art/ afterschool or combined with potential heritage building use. Commercial uses aligning with heritage status of the building such as art studio's, architectural or related businesses might also be considered. Such uses would provide the opportunity for the enclosed front garden to become a new, publicly accessible open space in the historic core of Clondalkin supporting and complementing the Round Tower site. There is also potential for the interior to be adapted to provide open plan spaces and functionality using contemporary design. Potential for re-development of this site in conjunction with adjoining site to the rear should the opportunity arise should also be considered.

Note: Due to uncertainty around flood extents the provisions of this section are conditional on the completion of the River Camac Flood Alleviation Scheme (FAS) until such time any development is premature.

#### The Old RIC Barracks

##### Parameters for Development

- ➔ Facilitate if opportunity arises the use of the enclosed front garden as a new, publicly accessible open space in the historic core of Clondalkin contributing to GI connections through the village centre.
- ➔ Explore opportunities for the re-development of this site in conjunction with existing potential development site to the rear, incorporating redevelopment of both sites with the Old RIC Barracks as centrepiece taking into account existing culvert traversing the site to the rear and potential risk of flooding on site.
- ➔ Ensure any future re-development is carried out in conjunction with works associated with any emerging VES.

#### Dutch Village

The Dutch Village Local Centre is located off Woodford Walk in Knockmitten, a primarily residential area to the southeast of the village core, within the outer environs of the Clondalkin LPF. The site directly adjoins church lands in private ownership to the west and public open space adjoining Woodford Avenue and Woodford Drive to the south and east. The site is zoned LC 'To protect, improve and provide for Local Centre Facilities' in the South Dublin County Development Plan 2022 to 2028.

While permeability and planting within the Dutch village are generally excellent, there are opportunities for small interventions which could support usability of the existing spaces and connections.

A number of local centres within the county have been redeveloped or have planning permission for redevelopment. While this is generally welcomed, any redevelopment must reflect its context within an existing suburban residential area.



Figure 8.16: Old RIC Barracks, Old Nangor Road: Architectural Impression



Figure 8.17: Dutch Village Location Map





Figure 8.18: Dutch Village

### Dutch Village Parameters for Development

#### Dutch Village Parameters for Development

##### Parameters for Development

- ➔ Explore opportunities as they arise to open a direct pedestrian/cycle connection from the car park to the adjoining open space lands at Woodford.
- ➔ Support the opening of the boundary wall between Woodford Drive and Monastery Heath as a Priority Permeability Route to shorten children's route to school and providing an alternative pedestrian route to Monastery Road.
- ➔ Assess any proposals for redevelopment of the existing buildings in the local centre against policy and objectives in the County Development Plan, ensuring that any such proposals have regard to their surrounding residential context.

### Water Tower

The disused Water Tower visible from Monastery Road is an asset and a potential way-finding device. Landmarks such as these can enrich the character of the built environment and public realm and contribute to the distinctiveness of an area. The Council will explore the potential to utilise this asset to provide greater legibility at this important gateway into the town centre. Lighting historical buildings and landmarks involves a thoughtful approach to ensure preservation, enhance aesthetics, and create a captivating nighttime experience, while also considering environmental impact and maintenance.

#### Water Tower Parameters for Development

##### Parameters for Development

- ➔ Investigate the potential to develop an art project at the disused water tower structure on Monastery Road as a southeastern landmark, signalling the entrance to Clondalkin Village Centre.
- ➔ Investigate opportunities to enhance the water tower structure as a way finding device associated with the village centre
- ➔ Safeguard potential opportunities to a future active travel link through this area to Knockmeenagh with the water tower and associated open space being a feature of such a link.



Figure 8.19: Water Tower Location Map



Figure 8.20: Water Tower Waymarking Example





## 8.6 Village Enhancement Schemes

The Village Enhancement Schemes (VES) present specific opportunities for high-impact projects to demonstrate the Local Planning Framework’s ambitions for Clondalkin. South Dublin’s capital programme includes a budget towards VES in Clondalkin and the LPF has ensured this has been considered as part of the urban design strategy. Two VES proposals are illustrated here in indicative form, with final determination of extents and implementation to follow at detailed design stage through a separate process which includes public consultation. At implementation stage the works may be phased in the manner considered most appropriate to their delivery.

### 8.6.1 Ninth Lock to Old Nangor VES

This VES is for a public realm proposal including cycling infrastructure for Ninth Lock Road from the Oakfield Junction opposite the Mill Shopping Centre, to the Junction with Mill Lane on the Old Nangor Road. It will include the Civic Plaza at the Clondalkin Civic Offices and the junction of the Old Nangor Road and the Tower Visitor Centre. A primary purpose of this VES is to achieve placemaking improvements on the approach to the Village from the north along the Ninth Lock Road. Of particular interest is the potential to improve on the blank facades presented by the Mill Shopping Centre to the Ninth Lock Road. This frontage is characterised by a long unbroken façade with narrow footpath fronting onto a wide heavily trafficked road. The VES can potentially achieve widening of footpaths, provision of segregated cycle lanes, added planting and SuDS features along the Ninth Lock Road. This would help mitigate the negative visual impact and poor current placemaking of the lengthy facade. Potentially it can also integrate with future improvement to the frontage of the Civic Plaza providing for example seating and planting, and improved cycle infrastructure. As the brownfield site to the north of the shopping centre develops there is opportunity to set development back to ensure that it complements the VES through provision of good cycle and pedestrian infrastructure and increased planting. Another primary purpose of the VES is to optimise road space along the Ninth Lock Road to provide for improved active travel infrastructure.

The VES for this area shall have regard to the following overarching policy objectives:

#### VES1: Ninth Lock to Old Nangor VES Overarching

##### VES1 Objective 1:

To support the preparation of a Village Enhancement Scheme (VES) from the northernmost point of the Mill Shopping Centre building, (opposite Oakfield) to the north, along the Ninth Lock Road, including the Civic Plaza, and to the junction with the Old Nangor Road at Round Tower Visitor Centre and west along Old Nangor Road to the junction with Mill Lane. The delivery of the VES may be phased.

##### VES1 Objective 2:

To support the rebalancing and redistribution of space within Clondalkin Village, notably from the Ninth Lock Road to the junction with Old Nangor Road, and along Old Nangor Road to the junction with Mill Lane, and towards the Old RIC barracks, to provide for improved active travel, urban greening, including nature-based SuDS features where feasible, and increased activation, making it more vibrant, engaging, and welcoming for all.

##### VES1 Objective 3:

To support better placemaking through measures to improve the animation of the Ninth Lock Road through increased soft landscaping, including nature-based SuDS features and trees where feasible, and providing additional crossing points and active frontages.

##### VES1 Objective 4:

To support the redevelopment of the Civic Plaza to provide an attractive urban space as part of a cohesive design with provision for an appropriate mix of hard and soft surfaced areas; de-culverting the River Camac where feasible or where not, the planting at ground level where the river is culverted, in the interest of representing the corridor by way of soft proposals; to provide a visually and functionally successful space, accommodating use by people of all ages and abilities, enhancement of biodiversity through urban greening including nature-based SuDS features.

##### VES1 Objective 5:

To support public realm improvements which will increase legibility of and increase activation along Old Nangor Road from the Mill Lane junction to the Ninth Lock junction through improved active travel provision, additional street planting, and durable, high-quality materials, complementing the materials around the Round Tower Visitor Centre.

Any future VES scheme for this area shall be guided by the following Design Parameters:

#### Ninth Lock Road to Old Nangor Road VES Design Parameters

##### Urban Design

- ➔ Facilitate the re-design of the Civic Plaza at the Clondalkin Civic Office as a key node within the village centre integrating it into the emergence of the Ninth Lock Road VES and active travel schemes
- ➔ To support and encourage improved urban design and placemaking, facilitating a visually attractive and welcoming urban village. To deliver guidance addressing street furniture, including bollards, to establish consistency, rationalise street features and remove features that contribute to clutter.
- ➔ To ensure a consistent approach to durable, high-quality materials complementing those already in place outside the Round Tower Visitor Centre.

##### Movement

- ➔ Create a public realm proposal for Ninth Lock Road and east of Old Nangor Road that improves the environment for active modes of travel.
- ➔ Fully integrate VES proposals with the Cycle South Dublin Programme.
- ➔ Prioritise pedestrian comfort and safety.

##### Climate Action and GI and Surface Water

- ➔ Increase tree provision and nature-based SuDS features, where feasible within the finite space needed to deliver on active travel improvements, vehicular movement and placemaking upgrades .
- ➔ Require new car parking provision and surfaces to incorporate planting and permeable paving.
- ➔ Where deculverting of the Camac is not feasible, delineate at ground level where the Camac is culverted, in the interest of representing the corridor by way of soft landscaping.

##### Wayfinding

- ➔ Provide consistent surface materials, planting and other wayfinding cues, including signage, within the VES area and across the village centre.

##### Activation

- ➔ Encourage ground-floor retail, restaurants, and other businesses that attract foot traffic and create a sense of activity along the street.
- ➔ Incorporate features such as outdoor seating, public art, landscaping, and street furniture that enhance the pedestrian experience.
- ➔ Ensure a consistent approach to street furniture within the VES and village area, in key locations, to facilitate the enjoyment and use of the village core and environs.



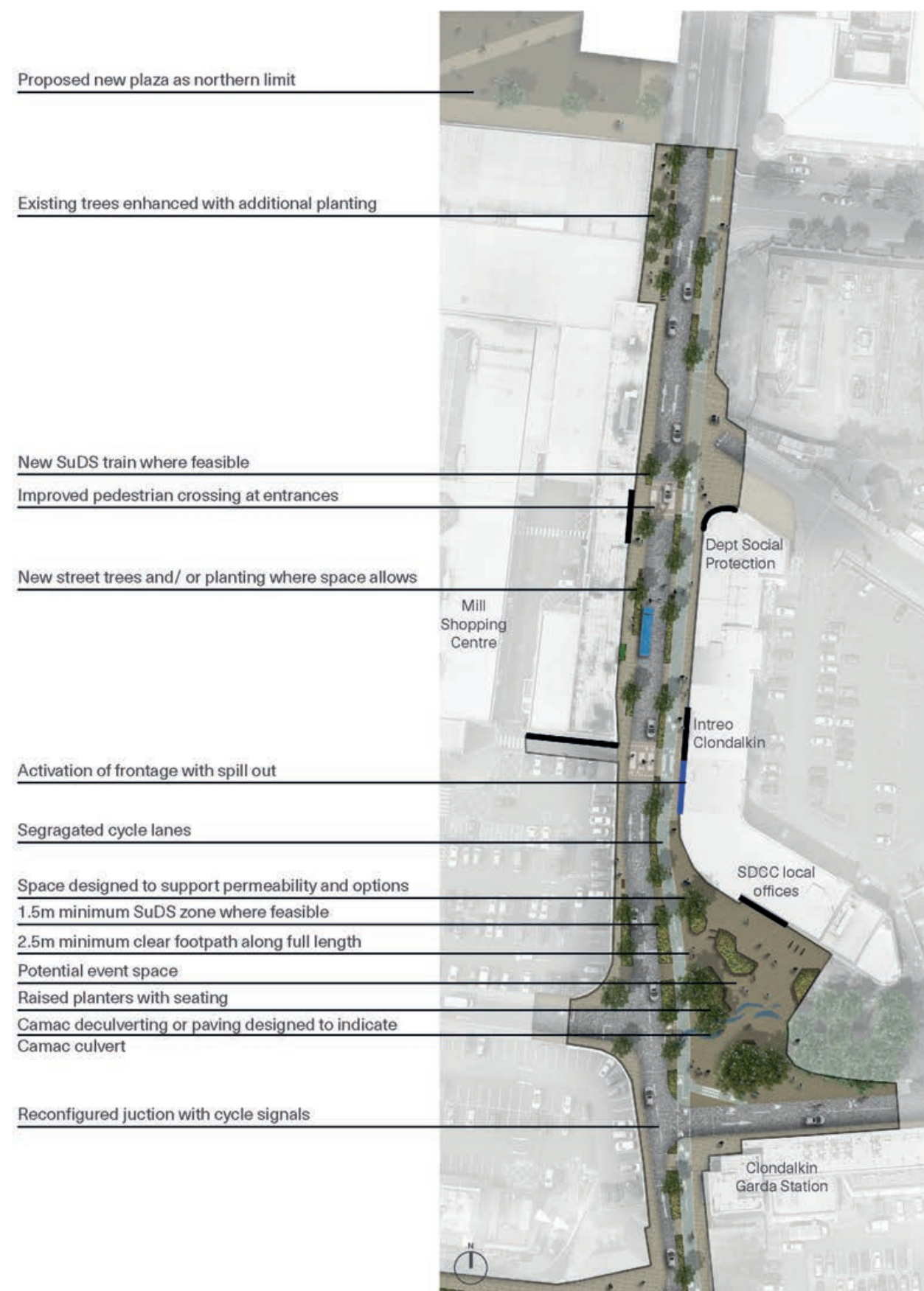


Figure 8.21: Ninth Lock Road VES indicative plan



Artists Impression of Civic Plaza

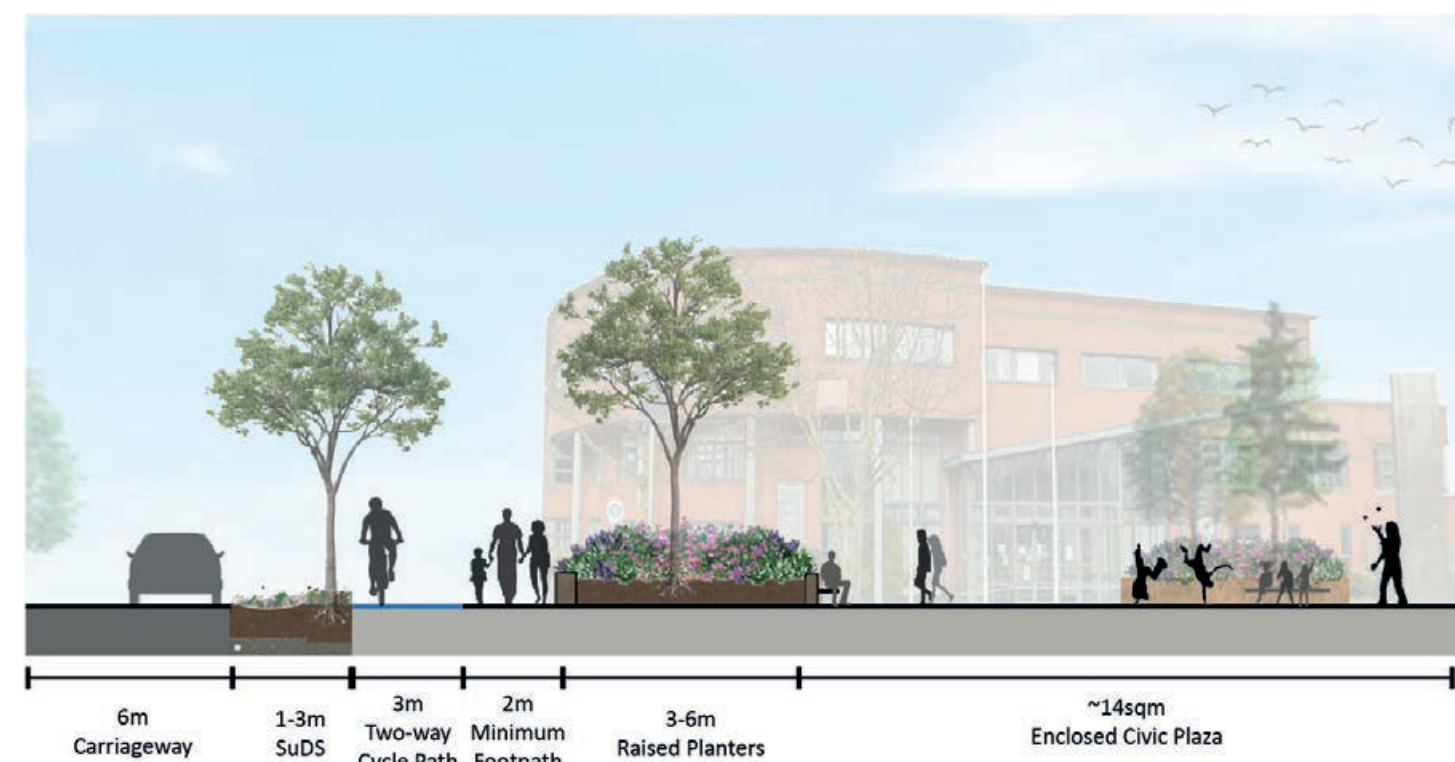


Figure 8.22: Ninth Lock Road VES Cross Section



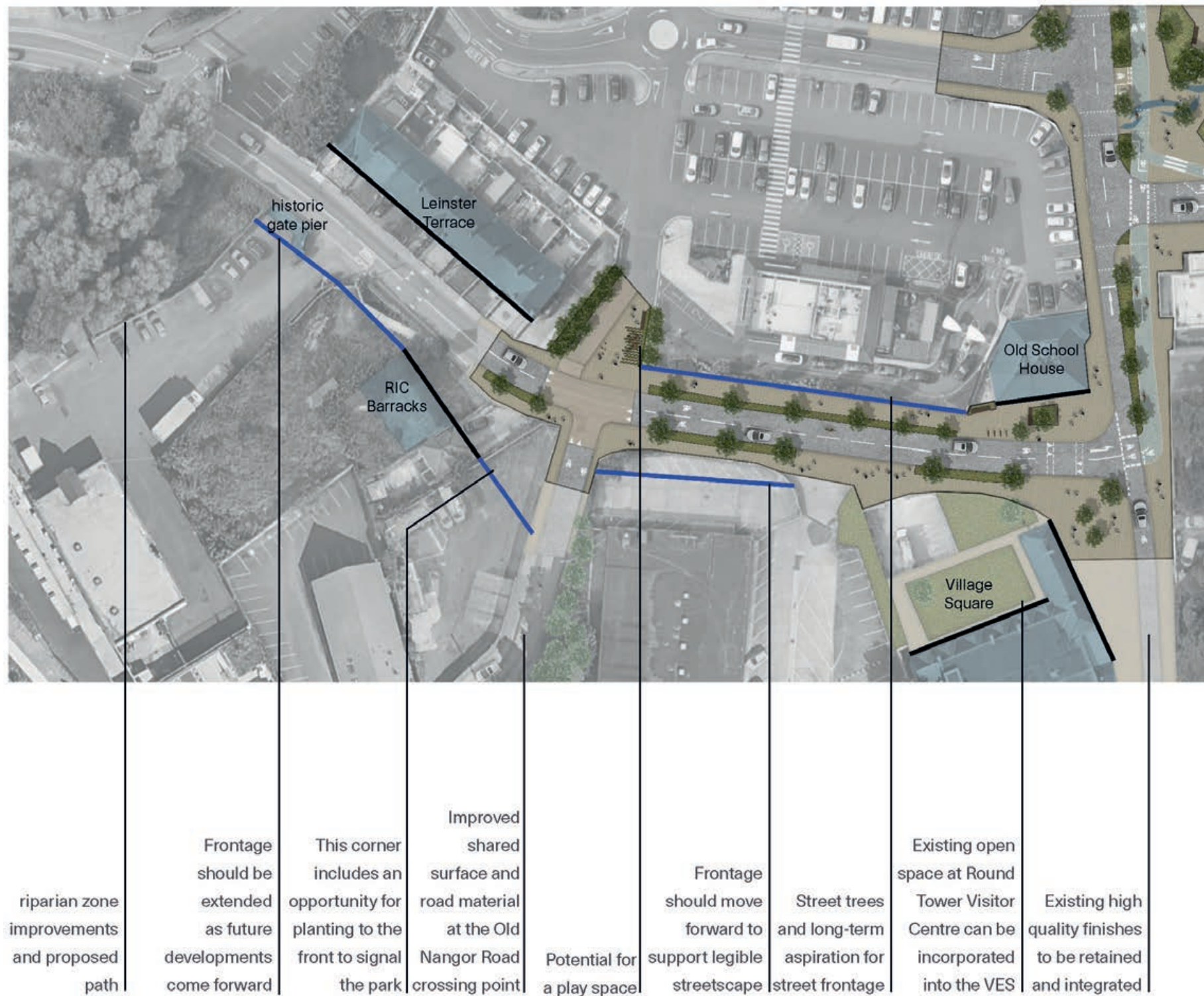


Figure 8.23: Old Nangor Road VES indicative plan



Figure 8.24: Artists Impression Ninth Lock Road Phase of VES

### 8.6.2 Main Street VES

Main Street VES involves a public realm proposal from Pope Lane/The Black Lion junction, along Main Street, to Tower Road. This project aims to let the historic streetscape dominate, through restrained application of materials throughout and decluttering and rationalising of street furniture. Its extents take account of the aspiration to create a new public entrance to Clondalkin Park from the village centre at the Tower Road Parade, and the potential for Cycle South Dublin routing through Pope Lane.

The VES for this area shall have regard to the following overarching policy objectives:

#### UD6: Main Street VES Overarching Objectives

##### UD6 Objective 1:

To support the preparation of the Village Enhancement Scheme (VES) from the junction of Tower Road, Main Street, along Main Street to the east as far as the Black Lion junction of Main Street / Orchard Lane / New Road (Laurel Park) / Monastery Road, encompassing Pope Lane. The delivery of the VES may be phased.

##### UD6 Objective 2:

To support the rebalancing and redistribution of space within Clondalkin Village, notably to Main Street and at the junction with Tower Road, Convent Road, Orchard Road and Laurel Park to provide for an improved pedestrian and cycle environment, urban greening and nature-based SuDS features where feasible, and increased activation, making it more vibrant, engaging, and welcoming for all.



Any future VES scheme for this area shall be guided by the following design parameters:

### Main street VES Design Parameters:

#### Urban Spaces

- Ensure that the VES is designed to signal the Black Lion junction as a historic junction of significance to the Village Centre.
- Improve the current environment for pedestrians through reimagining of currently underutilised space within the public realm.
- Ensure the VES benefits businesses through an Improved public realm, with appropriate street furniture where space allows (e.g. seating) encouraging greater footfall and street activity.
- Investigate opportunities to reorganise car parking within the Tower Parade to the benefit of the public realm and wider village environment.

#### Movement

- Investigate and promote further opportunities for permeability through the Town Centre 'oval'.
- Integrate Cycle South Dublin schemes, both east-west and north-south through this critical junction, examining the potential for Pope Lane to better support active travel movement.
- Consider as part of any village improvement scheme, the need for loading bays to facilitate the businesses and traffic movement in the village centre

#### Climate Action and GI

- Increase tree provision and nature-based SuDS features, where there is sufficient width to incorporate planting, pedestrian and cyclist movement alongside vehicular movement.
- Provide new urban street tree planting at appropriate locations to help improve local air and water quality. Require new car parking provision and surfaces to incorporate planting and permeable paving to ameliorate the impact of pollution and surface water runoff.

#### Wayfinding and Legibility

- Ensure that the VES incorporates opportunities to improve wayfinding into the village from Convent Road and Boot Road.
- Incorporate wayfinding features to signal the junction of Main Street and Tower Road as the southern entrance to the village core.
- Ensure a consistent suite of wayfinding and legibility design features for the whole village and surrounds.

#### Materials and Finishes

- Ensure high quality durable materials and a consistent approach to VES throughout the village which complements the existing public realm improvements at the Round Tower Visitor Centre.

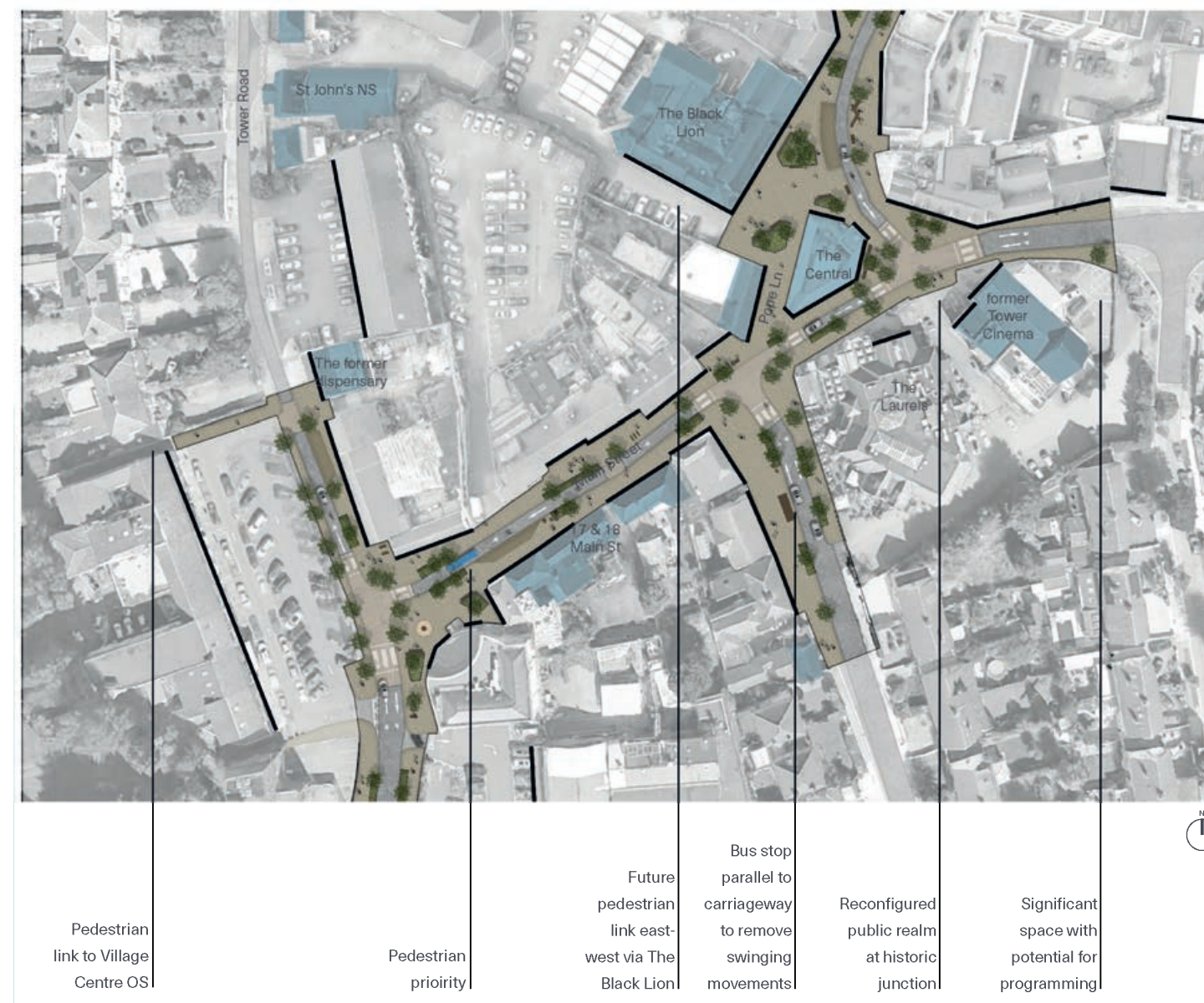


Figure 8.25: Main Street VES



Figure 8.26: Artists Impression Main Street VES





## 8.7 Large Scale Development Frameworks

The identified large scale development sites, which represent the areas of significant transformational potential within the Clondalkin Local Planning Framework (LPF) boundary are:

### 8.7.1 Ninth Lock Road Framework Site

The Ninth Lock Road Framework Site comprises lands located to the north and west of the Mill Shopping Centre (west of the Ninth Lock Road) and to the south of the New Nangor Road extending to approximately 7 hectares.

The framework site represents the largest opportunity for new residential and mixed use in the town centre, with capacity for 1,000+ new homes, alongside associated amenity and open space, community facilities, town centre frontage and green infrastructure.

Development of this site will play a crucial role in delivering the wider objectives of the Urban Design Strategy, including connecting the core village area and ACA, open space network and the planned Ninth Lock Road Village Enhancement Scheme. With an approximate future population of 2,400 people this framework site contains a significant element of the future population growth for Clondalkin.

Guidance is provided here for future development to enact these positive contributions to village centre life. Proposals should have reference to the four key indicators of urban design performance identified by the DHLGH Sustainable Residential Development and Compact Settlement Guidelines, and should develop contextual responses in line with the South Dublin County Council Building Height and Density Guide.

Range	Ninth Lock Framework Area
Zoning	TC (Town Centre)
Site Area	c. 7 HA in two parcels.
Density Designation	City – Urban Neighbourhood
Density Range	50dph – 250dph
Potential Residential Development Capacity	1000+ dwellings alongside associated open space, amenities and services/facilities with other mixed uses.

The following development parameters provide a site-specific focus to the objectives in the preceding chapters. Objectives within those chapters remain relevant and should be referred to alongside the development parameters in the preparation of any development proposals and their assessment.



Figure 8.27: Location of Ninth Lock Road Framework Area within the LPF





**Figure 8.28: Aerial View of Ninth Lock Framework Site**

#### Ninth Lock Framework Site – Development Parameters

##### Placemaking:

- Demonstrate how development has had regard and responded to the design principles, including urban grain, set out in this section for the framework site.
- Ensure that development along Ninth Lock Road is designed to provide for an attractive approach to the village centre at a scale which facilitates the integration of new development along this road frontage with the village centre.
- Ensure the provision of appropriately sized and purpose-built community facilities as part of any future development of this site, locating it to facilitate good placemaking along the Ninth Lock Road and the integration of new and existing communities with the village centre. See Chapter 6, CHE9 Objective 1
- Ensure that the delivery of development fronting the Ninth Lock Road including the community space is phased in such a manner that it commences prior to the completion of 250 units within the site or as may otherwise be agreed with the planning authority.
- Provide for a civic plaza along the Ninth Lock Road – ideally this will be integrated with the community centre to provide for indoor and outdoor community uses.
- Provide for a landscaped ‘landing space’ on arrival from Old Nangor Road into the development.
- Deliver a mix of housing typologies to meet the needs of different age and family cycles, including housing for older people where such need is identified, to ensure a sustainable community.
- Ensure the appropriate provision of recreational facilities, with potential for some to be located within the pylon corridor alongside green infrastructure, based on an assessment of need and agreed with South Dublin’s Public Realm section as an integral part of any development on this site.
- Consider views of the Round Tower and church spires from Dunawley Park south through the framework site and demonstrate how they have been incorporated into the design of any future development.

#### Ninth Lock Framework Site – Development Parameters

##### Movement

- Encourage servicing to the Mill Shopping Centre to be carried out by way of access through a new road network within the Framework site, to help reduce commercial traffic movement within the village centre.
- Ensure permeability throughout the site including:
  - Active travel link to / from the Old Nangor Road
  - Active travel links to Ninth Lock Road through an east-west connection.
  - Active travel link to the car park at the Mill Centre Shopping site
  - Active travel link/s to the New Nangor Road facilitating access to bus stops and to Fonthill train station by way of Dunawley Park open space
  - Vehicular link to New Nangor Road and Ninth Lock Road
  - Potential for future vehicular link to the Mill Centre should it be required in the future
- Retention of the high-capacity transport reservation and provision of active travel links along it ensuring it is sufficiently wide for future accommodation of any planned transport.
- Ensure sufficient setback along the Ninth Lock Road to provide for cycle lanes, street trees, widened footpaths in addition to the existing bus lane along this road.
- Provide continuance of the existing footpath from Mill Park to the west of the site from the New Nangor Road to connect to the Ninth Lock Junction.
- Liaise with the NTA to investigate the positioning of bus stops and potential for any additional bus stops along New Nangor Road and the provision of bus shelters.

##### Green Infrastructure

- Deliver public open space in accordance with the standards set out in the County Development Plan.
- Create pockets of green space designed to increase biodiversity
- Provide for a landscaped ‘landing space’ on arrival from Old Nangor Road into the development.
- Create an east-west GI connection through the site.
- Facilitate improved GI connections between Clondalkin Park and Dunawley Park to the north.
- Require street tree planting and ensure that it is within the public realm.
- Retain the existing mature treeline and hedgerow along the western boundary of the site and establish a pedestrian / homezone route alongside.
- Ensure that a SUDS Strategy is provided for any development on the site in line with South Dublin County Council Sustainable Drainage Explanatory Design & Evaluation Guide and which includes bio retention tree pits as part of street tree planting.
- Ensure that development meets the Green Space Factor requirements of the County Development Plan

#### Ninth Lock Framework Site – Development Parameters

##### Infrastructure

- Ensure that where contamination is present it is sufficiently identified and remediated prior to any development on relevant sites as part of any phasing requirement.
- Investigate the feasibility of undergrounding the existing pylon corridor in the northeast of the site. In the absence of such feasibility to ensure that development appropriate to any safety constraints is facilitated including potential recreational facilities and green infrastructure.

#### Contextual Analysis

An outline contextual analysis of the Ninth Lock Framework Site is provided here to identify design drivers for a contextually responsive and appropriate form of development for future proposals, following the methodology of the SDCC Building Height and Density Guide 2022 (Appendix 10 of the CDP).

#### Context

The lands are serviced by a frequent bus service, which will be improved with the introduction of BusConnects. The routes provided at this location include the 60, 68, L54 and W2. Buses also serve the train to the north and the LUAS at Red Cow, as well as providing services to the Quays, Grange Castle, Liffey Valley and Tallaght. The Ninth Lock Road is also identified as a Cycle South Dublin (CySD) route, connecting Tallaght to Clondalkin towards the Canal, to provide for improved cycle infrastructure.

The framework site is an appropriate location for local intensification and a Metropolitan Landmark (see SDCC Building Height Guidelines) to signal the northern entrance of the village. The western edge of the framework site is adjacent to finer grain historic and contemporary housing and should respond to these. Across the 400 + m of the framework site proposals will need to reconcile this difference in context.

#### Setting

The context of the framework site is predominantly vehicle dominated wide roads with a lack of active frontage and permeability. The framework site provides opportunity for significant benefits to this context by changing the dynamic, breaking down the block size, introducing increased permeability and active frontages on appropriate streets, including on the Ninth Lock Road, drawing from the historic grain of the Village.

#### Connections

The framework site is bisected by the 30m reserve for high-capacity transport (shown as Metro reserve on Figure 8.29). This is ideally aligned to provide a direct route through the site to Clonburris National School and onwards to Clondalkin – Fonthill station from the village centre. The site also contains a pylon corridor which will provide a green link between Oakfield and Dunawley Park. Allowance should be made for future connectivity into a redeveloped Mill Centre, should that happen.





### Inclusivity

Primary active frontages onto the Ninth Lock Road can help improve the environment of the village centre, with internal frontages with residential own door units providing a more local context.

### Variety

From the finer grain of the western boundary, across the ~430m to the Ninth Lock Road, there will be at least 3-4 urban blocks and an opportunity to introduce variety of typologies, massing and materiality. For example, this could vary from own-door duplexes on the Mill Park edge, through medium-higher density residential apartment blocks, to mixed-use and commercial frontages with apartment above on Ninth Lock Road.

### Efficiency and Distinctiveness

Given the Town Centre zoning and proximity to public transport density will generally need to be towards the upper end of the city-urban neighbourhood density range, subject to further detailed analysis at planning application stage.

The framework site offers an opportunity to introduce a new urban grain into a part of Clondalkin which has long remained undeveloped. The character of narrow streets and narrow frontage widths can be introduced through this site, as a way of reflecting the local character.

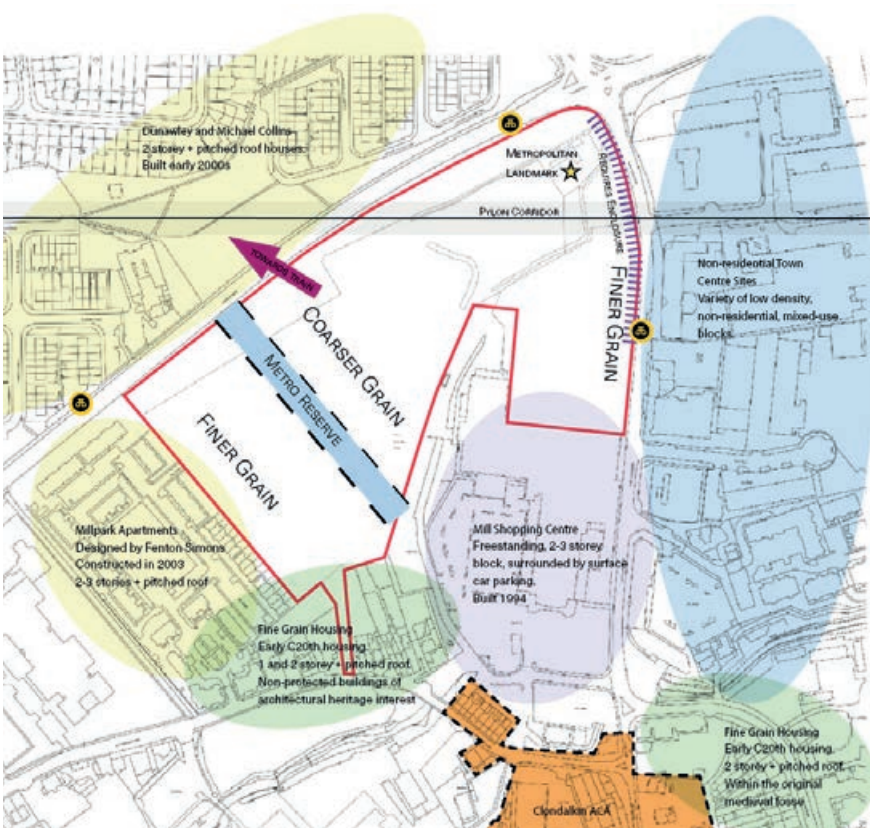


Figure 8.29: Contextual Analysis

The northern gateway should be distinct and support wayfinding to the village. This could include additional height at this corner, transitioning to the village/town centre scale over the 250m of Ninth Lock Road frontage.

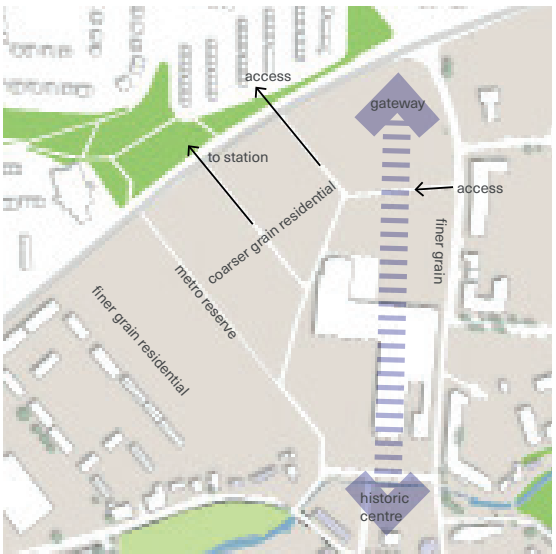
### Layout

The layout should prioritise frontage and enclosure onto all existing and proposed streets. This includes making provision for future redevelopment of the Mill Centre site.

### DESIGN PRINCIPLES



01 The receiving environment lacks permeability.



02 The first step is to break up the land and establish a new urban grain.



03 The blocks are further broken down into development plots and pedestrian connections.



04 The public realm is defined: green links, green open spaces, street trees and SuDS, public plazas.



05 Frontages are identified.



06 Plots can be infilled with appropriate buildings, delivering finer grain frontages, activity, mixed-uses and a diversity of new homes.

Figure 8.30: Design Principles

### Public Realm

The framework site can deliver a custom public plaza appropriate to the scale of Clondalkin on Ninth Lock Road. This space can be designed to suit the needs of the growing population, to host markets, festivals and local cultural events. Ideally this space will be activated by a community use. The framework site will also support GI with a new stepping stone park as part of the wider requirement for public open space and green infrastructure.



## Adaptability and Parking

Given the TC zoning and location, it would be appropriate to reduce car parking and podiums. Parking should be reduced throughout the development, giving opportunity to introduce local accesses through perimeter blocks.

## Detailed Design

Significant opportunity for refinement will be available through detailed design. The design parameters here aim to establish the urban grain and overall character.

## Streetscape Analysis

Below are indicative site sections / elevations demonstrating activity, grain and connections.

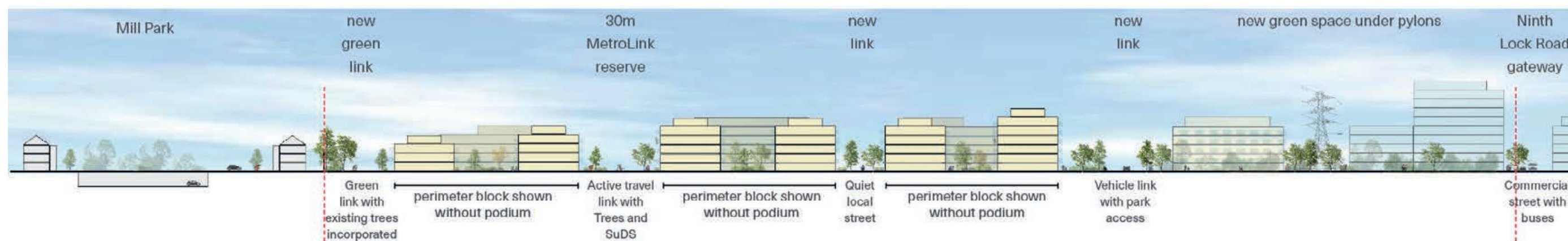
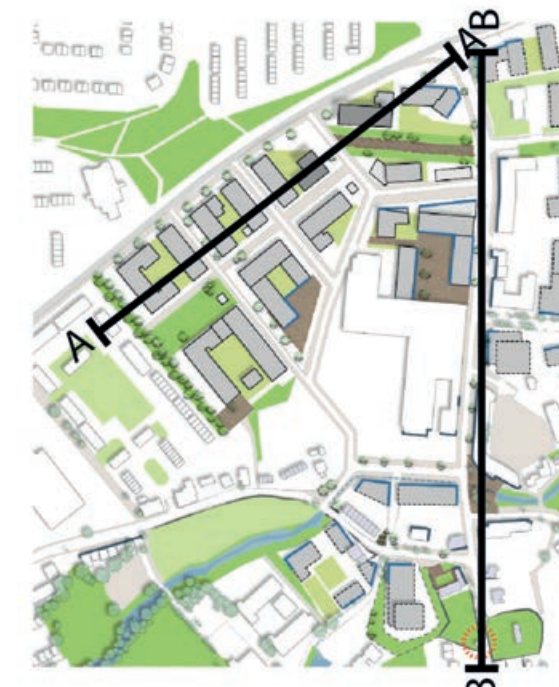
### AA-AA: East-west section

Transitioning from the low density, low-rise context of Mill Park on the western edge to the commercial frontage and landmark of Ninth Lock Road can happen gradually over 3 to 4 urban blocks, with a diversity of homes and streets in between.

### BB-BB: Northern Gateway – North-South Section

From the New Nangor Road junction to the Round Tower Visitor Centre is approximately 600m (10 mins walk). The framework site can positively influence the first stretch of this journey, with activity and enclosure, as well as street trees, SuDs and high-quality footpaths and cycle lane.

## Cross Sections



Figures 8.31: Streetscape Analysis: Cross Sections

### BB-BB: East-west section



Figures 8.31: Streetscape Analysis: Cross Sections





Figure 8.32: Ninth Lock Road Framework Indicative Layout.

← - - Vehicle movement

← - - - Active movement

— Active frontage

**Figure 8.32 Key:**

- a. Retained and enhanced tree line with active link
- b. New public open space (urban park)
- c. Density closer to mid-range of city – urban neighbourhood reflecting context of adjoining site, and finer grain housing blocks at this location subject to detailed design
- d. Pedestrian connection to Old Nangor Road with ‘landing’ green when entering from Old Nangor.
- e. Housing blocks at higher density than to the west, within city – urban neighbourhood range, medium grain, housing blocks.
- f. Potential for open space and activity which could integrate with any future development on adjoining Mill Centre site.
- g. Active link and green spine under pylons
- h. Pedestrian crossing to open space at Dunawley Park facilitating links to Fonthill train station
- i. Landmark building
- j. Re-opened vehicle connection
- k. Double fronted building with active community use at ground floor.
- l. Green link under pylon potential to link eastwards within Oakfield
- m. Pedestrian connection towards Clondalkin Park.



## Adjacent Lands and Northern Gateway

The northern edge of the Ninth Lock Framework Site is bounded by the New Nangor Road to the north and the Ninth Lock Road to the east and serves as an important gateway to the village.

Any future development on this part of the framework lands to be guided by the following overarching considerations and principles:

### Northern Gateway – Parameters for Development

- Support the South Dublin County Council tourism strategy to encourage appropriate wayfinding from the Grand Canal to the Village centre.
- Support wayfinding and strengthen legibility through an appropriately designed landmark building which responds appropriately to SDCC building height guidelines at the entrance to the village centre.
- Mark the entrance to the village encouraging reduced vehicle speeds and facilitating safer cycle and pedestrian movement within the village through improvements to the junction of Ninth Lock Road with New Nangor Road.
- Improve the streetscape and public realm at this junction in as part of any future development.
- Ensure that development complies with the Planning System and Flood Risk Management Guidelines for Planning Authorities and the SFRA accompanying this plan.
- Ensure that, where the electricity network cannot be undergrounded, development complies with the reservation distances required for safety or other reasons underneath the pylons and provides for an active travel route and greening within the reservation area as a minimum, with potential for recreational facilities.
- Ensure that where sites are developed independently on either sides of the junction (east and west), consideration should be given to how they would be reflected on the opposing side of the road, ensuring that provision is made for a coherent design approach to reflect the northern entrance to the village.

Subject to flooding and related assessment, there is also potential for development on the eastern side of the junction. Reference to a metropolitan landmark building applies here also and many of the parameters for development around wayfinding and legibility, junction improvements also apply. Should sites be developed independently on both sides of the road, consideration should be given to how they would be reflected on the opposing side of the road, ensuring that provision is made for a coherent design approach to reflect the northern entrance to the village.



Figure 8.33: Aerial Image of Northern Gateway





### 8.7.2 Knockmeenagh Framework Site

Lands within the Knockmeenagh Framework Site are zoned for employment with residential zoned land containing St. Brigid's ACA in the centre. From the west, Joel's Restaurant and the Louis Fitzgerald Hotel form the boundary with the residential context to the west. They are adjacent to low rise, light industrial units. Between this light industrial and St. Brigid's lies a large green field which has been subject to unsuccessful planning applications in the past. St Brigid's itself is made up of very low density single storey housing running north-south. The eastern end of the framework site returns to the light industrial employment character, much of it related to logistics and underutilised sites, with an office block on the junction with the Joe Williams Bridge / L1019.

Further mixed industrial lands lie within the small triangle of land further east between the L1019, the N7 and Monastery Road.

Knockmeenagh Lane to the north of the framework lands has the potential to improve active travel routes and permeability and better integrate the framework site, St Brigid's ACA and the Luas with the central area of the village. There is currently full access through the lane, but it has no overlooking, is poorly lit and the surface finish varies. Development principles described in this section will give guidance as to how these issues can be remedied through future redevelopment.

#### Challenges for Development

As described above, this framework site is zoned for employment with the exception of the lands at St. Brigid's cottages. Typically, the demand for employment use along the N7 has been in the form of logistics and retail or car showrooms. Notwithstanding the different context with the presence of an ACA between the employment lands, the presence



Figure 8.35: St. Brigid's cottage within ACA.



Figure 8.34: Extract from CDP 2022-2028 zoning map as adopted in June 2022.

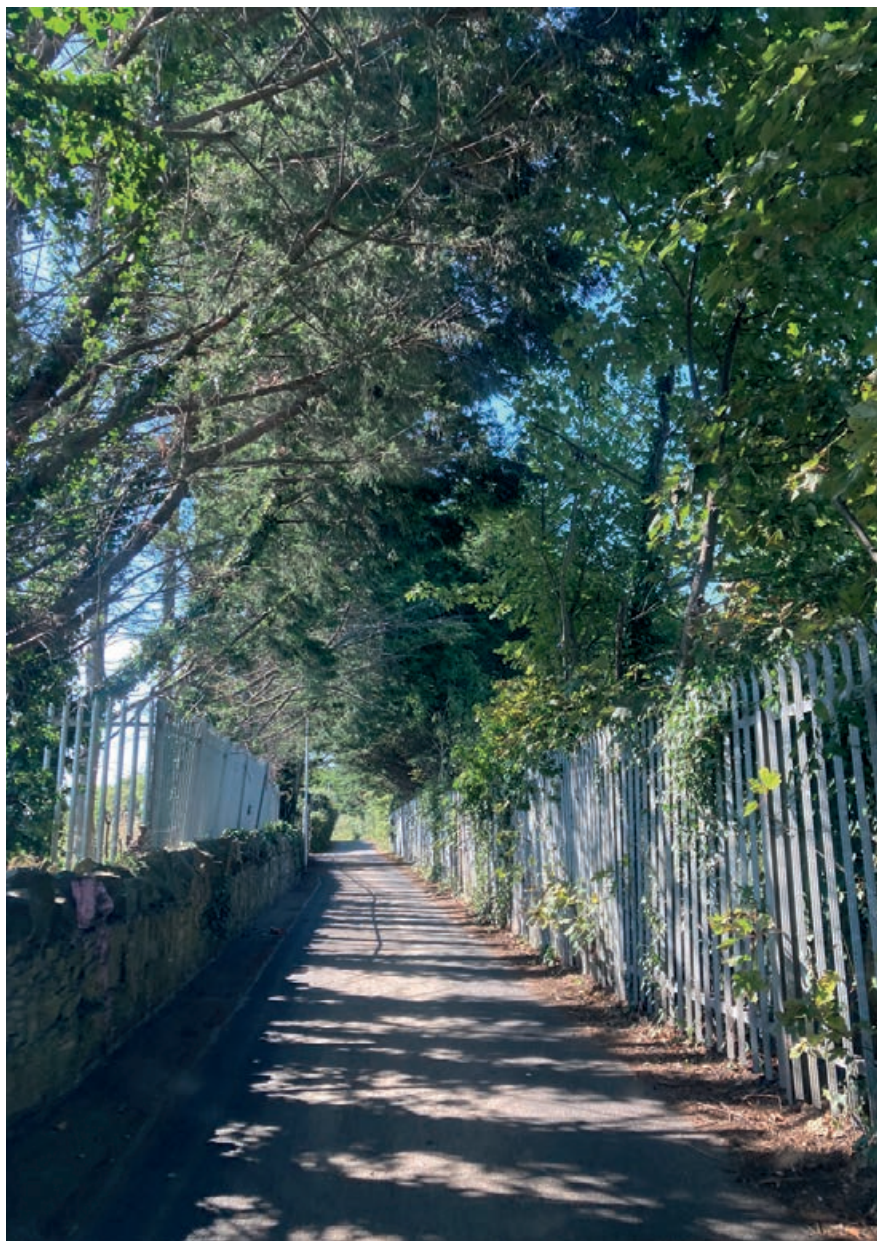
of the N7 to the south and the associated slip road between it and the framework lands has led to difficulties in developing the lands to their full potential. Discussion as part of the preparation of this LPF with TII has indicated that no further access points can be accommodated along this stretch of road and ideally the number of current access points would be reduced.

Knockmeenagh Lane and Road enclose the lands on their northern boundary. As set out in the Conservation Plan and Chapter 7 of this document, the lane is recognised as part of an old pilgrim route, a Slí. As the road narrows at its eastern end its use is appropriate for walking and cycling only. Given its historic context, this is unlikely to change. At the same time, the link between Knockmeenagh Lane and Monastery Road is sub-optimal for active travel in terms of its width, surface materials, boundary treatment and safety. There are opportunities to improve this as part of any future development while protecting the Slí and historic walls.

At its more western end, Knockmeenagh Road is residential in context. Having regard to that context it would not be appropriate for an intensification of industrial type vehicles to exit onto the road or lane towards New Road and onwards to the N7.

Given the constraints in the current road infrastructure, and the lack of alternative solutions to facilitate the current employment zoning it is not intended to progress this framework site at this time. However, the LPF has considered whether a form of mixed-use development within the site would facilitate a more efficient and appropriate use of land. The following sections examine the contextual analysis and the possible design principles should the lands come forward for employment development as currently zoned, but more specifically should the lands be considered for rezoning as part of a separate variation to the CDP.





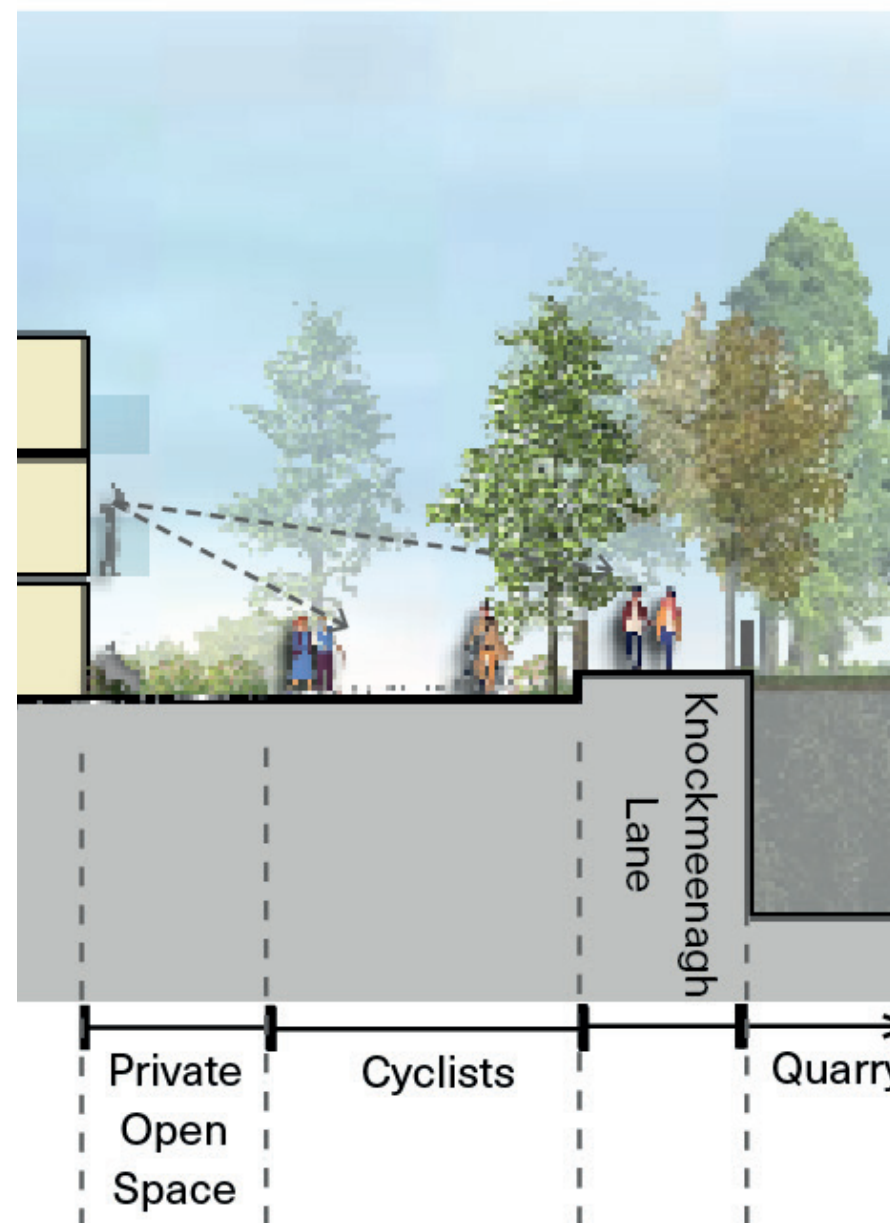
**Figure 8.36:** Opportunity to improve access along Knockmeenagh Lane as part of any future development while protecting its historic integrity – indicative only.

### Contextual Analysis

An outline contextual analysis of the Framework Site is provided here to identify design drivers for a contextually responsive and appropriate form of future development, following the methodology of the SDCC Building Height and Density Guide 2022.

### Context

Connectivity: the lands are serviced by buses 68, 13, 60, 69, L54, G1. These services will further improve with BusConnects. The Luas, bus interchanges and Park and Ride at the Red Cow LUAS are approximately 950m from St Brigid's Cottages with current connections. National bus services can be picked up from the lane at St. Brigid's.



Intensification: The framework site could be an appropriate location for local intensification because of its location adjacent to the Luas and the potential for greater active travel linkages. However, as it stands its location adjacent to the N7 provides roadside frontage but limited road access and significant noise pollution.

### Setting

Contextual response: The immediate context of the framework site is mixed. To the west and north are low density residential areas with minimal permeability. To the north east there is a former limestone quarry and while no longer operated as a quarry the lands are in continued employment use for bitumen manufacturing. The N7 boundary to the south provides limited access and significant noise levels.

The framework site can provide positive benefits to the context by increasing permeability towards the Luas, breaking down the block size and providing overlooking onto Knockmeenagh Lane and its associated active travel link. St Brigid's ACA in the centre would also benefit from an improved setting.

### Connections

The framework site is bisected by St Brigid's ACA. The setting of this residential lane is undermined by its N7 access. Proposals should offer an alternative and more appropriate residential access to St Brigid's Cottages. This would further allow Knockmeenagh Lane to downgrade to active travel only along its more easterly section.

Improved permeability through the framework site would support access to public transport for adjacent residential areas also.

### Inclusivity

Increased population and overlooking onto Knockmeenagh Lane would support this active travel connection. A central open space would also help offer play opportunities for all ages and support improvements to green infrastructure in this part of Clondalkin.

### Variety

A finer grain along the northern edge, overlooking Knockmeenagh Lane, across the approximately 210m to a coarser grain non-residential edge onto the N7, provides potential for 3-4 urban blocks and an opportunity to introduce a variety of typologies, massing and materiality. This might include own-door duplex blocks, apartments and larger footprint light industrial uses. To achieve this type of potential the lands would need to be rezoned.

### Efficiency

Given the location of the framework site generally within 1,000m of a high-capacity public transport interchange, subject to improved connections, there is the potential for a review of the zoning at this location to make greater use of the lands. Should rezoning be considered in the future, the presence of the ACA and the different context throughout the site will determine the appropriate density within different areas of the site in accordance with the relevant guidelines.

### Distinctiveness

Any proposals must protect the integrity of St Brigid's Cottages ACA. The ACA could benefit from a buffer of mews style development where opportunity allows at the ends of the existing long gardens. However, some piecemeal development has occurred within some back gardens and achieving a cohesive mews style development may prove difficult. However, it could be facilitated by provision of improved access ensuring the existing plot scale is preserved. The integrity of Knockmeenagh Lane and its historic setting must be preserved but opportunity to improve the active travel link it provides should be factored into future development, Figure 8.36 above gives an indicative understanding of a potential solution without impacting on the lane's integrity.



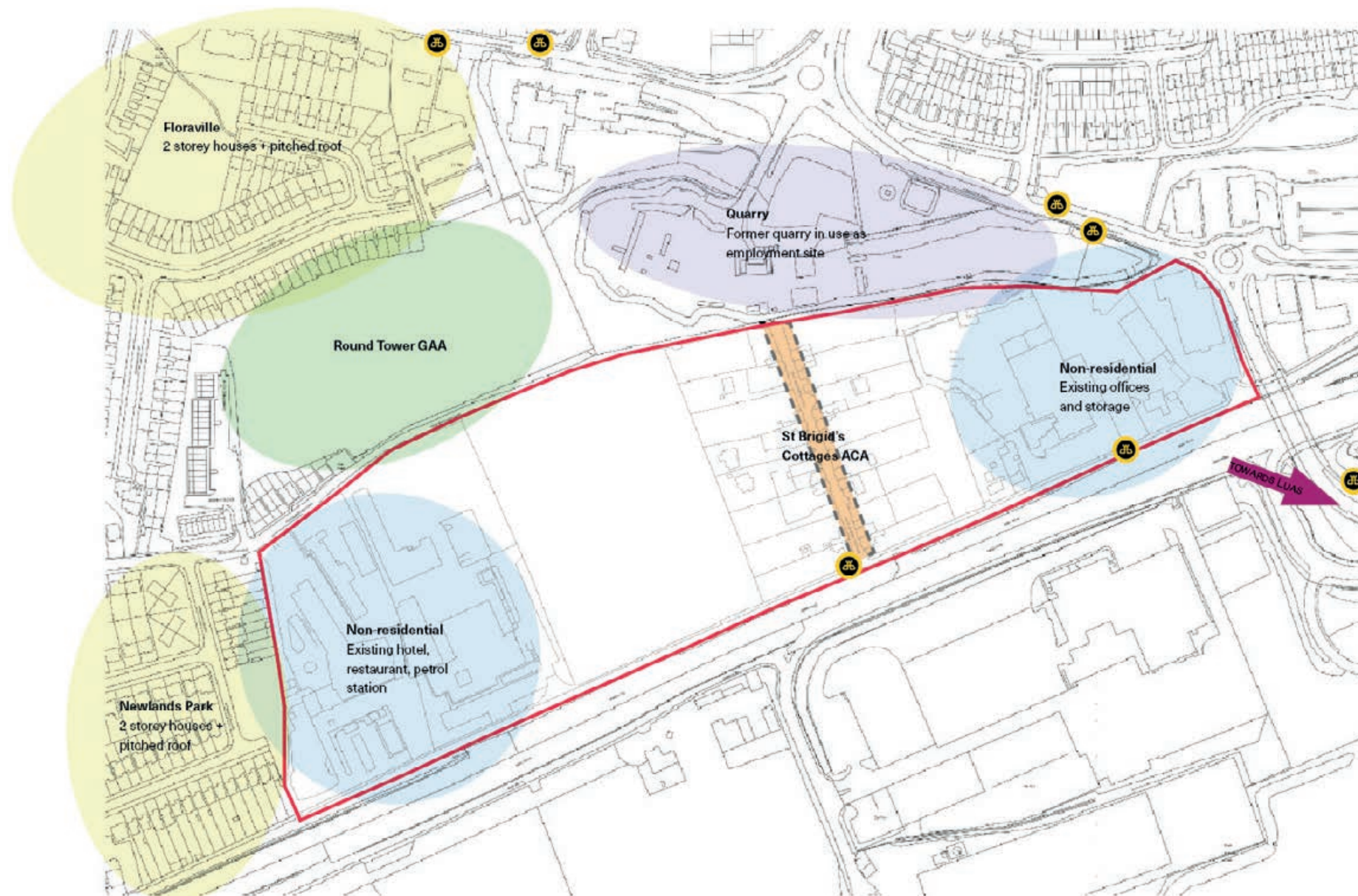


Figure 8.37: Land Uses within and around the framework site

### Layout

The layout of future development should prioritise frontage and enclosure onto all existing and proposed streets.

### Public Realm

The framework site should provide for a new central open space, which is much needed in this part of Clondalkin. The existing active link at Knockmeenagh will also benefit from improved overlooking.

### Adaptability

Given the proximity to high-capacity public transport it would be appropriate to reduce car parking and podiums.

### Privacy

There are limited close neighbours to the framework site which would be at risk of overshadowing or overlooking and this could be dealt with as part of detailed design of future development at planning application stage.

### Parking

Given the proximity to the Luas and the potential for better connections through the site as part of cohesive development, consideration would be given to reduced levels of parking in line with CDP standards.

### Detailed Design

Significant opportunity for refinement will be available through detailed design. The design parameters here aim to establish the urban grain and overall character of any future development.

### Design Principles

The primary purpose of the design principles is to demonstrate how good design and layout could maximise the potential use of the lands in a scenario where mixed-use development which is residential led is provided for. This can only be achieved where a separate variation on zoning to the CDP is approved.



01

The receiving environment lacks permeability.



02

The first step is to break up the land and establish a new urban grain.





03

The blocks are further broken down into development plots and pedestrian connections.



04

The public realm is defined: green links, green open spaces, plazas.



05

Frontages are identified.

- Non-residential active frontage.
- Primary Residential frontage.
- Secondary frontage.

## Knockmeenagh Objectives

Development within the Knockmeenagh Framework area and supporting street network shall be guided by the following overarching objectives:

### Objective KF1 Zoning and Transport

KF1 Objective 1: To include the Knockmeenagh Framework site lands as part of the review of zoning for new housing targets and land capacity within the County.

Objective KF1 Objective 2: To continue to liaise with TII and NTA to work towards long-term transport solutions which would facilitate an efficient use of the lands given the context of the surrounding road infrastructure and its constrained capacity to absorb different types of development.

### KF2 Design Parameters

KF2 Objective 1: To ensure that any proposed development on the lands is legible and permeable and does not compromise future potential of the lands having regard to the design principles set out in this plan.

KF2 Objective 2: To ensure that any development has the necessary regard to context as provided for in South Dublin's Height and Density Guide 2022 (Appendix 10 of CDP) and, where in proximity to St. Brigid's cottages demonstrates an appropriate design response to the existing single storey cottages.

KF2 Objective 3: To promote a strong edge to the framework lands along the N7, including elements of planting where feasible.

KF2 Objective 4: To ensure that development adjacent to Knockmeenagh Lane provides active frontage to allow for passive surveillance of the lane, incorporating new planting and providing for improved active travel infrastructure to complement the existing infrastructure

### KF3 Connectivity

KF3 Objective 1: To protect the historic integrity of Knockmeenagh Lane and associated Slí Mor while supporting sensitive solutions to its improvement as an active travel route along its existing connection from Monastery Road to New Road, providing for potential new connections to and from the framework site to the Lane and further northwards.

KF3 Objective 2: To explore the potential for delivery with relevant landowners of a pedestrian and cycle link from Knockmeenagh Lane to Monastery Road by way of the western boundary of the SIAC site and Round Tower GAA club.

KF3 Objective 4: To improve wayfinding and legibility from Monastery Road to Knockmeenagh Lane through appropriate signage and public realm improvements, where feasible.

KF3 Objective 5: To safeguard the long-term delivery of an active travel link across the quarry, towards Knockmeenagh Lane.

### KF4 Green Infrastructure

KF4 Objective 1: To improve the ecological GI value and connectivity of landscape features created as part of any new development.

KF4 Objective 2: To require all new development to demonstrate the integration of ecosystem services including nature-based SuDS as part of the design rationale.